



HARRISON COUNTY ACTIVE LIVING INITIATIVE

FINAL June 2022



The Committee acknowledges the members and agency staff who provided valuable input throughout the study:

PREPARED BY:



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EXECUTIVE SUMMARY

PROJECT BACKGROUND

The Harrison County Active Living Initiative (the Initiative) develops a long-term vision as well as specific action steps for a more active, healthy, and livable county. The planning process involved in the Initiative asks key stakeholders to envision what could encourage residents to be more active every day. This vision guides the specific network and policy recommendations specified in the Initiative.

VISION FOR THE HARRISON COUNTY ACTIVE LIVING INITIATIVE

Developed through a series of stakeholder meetings and interviews, the vision for active living in Harrison County is:

Harrison County will be a place in which incorporated and unincorporated areas are connected through a safe and engaging active transportation network that encourages a healthy, active lifestyle for residents and visitors alike.

PROJECT GOALS

- **Build out Harrison County’s multimodal network.** Plan, implement, and maintain a comprehensive, connected, and efficient network of comfortable, safe and inviting sidewalks, bikeways, trails, and Complete Streets that link neighborhoods and activity centers, and provide convenient access to the widest range of destinations. Pedestrian and bicycle facilities will be designed to provide an accessible, low-stress experience for users of all ages and ability levels.
- **Reduce the frequency and severity of bicycle and pedestrian crashes** through the implementation of systematic safety improvements and street designs that prioritize safety for vulnerable road users.
- Incorporate practices that **enhance equitable distribution** of active transportation opportunities.

THE INFORMATION GATHERING PROCESS

- Reviewed relevant plans and policies
- Analyzed data on demographics, employment, road safety, and access to key destinations
- Gathered input from community members via virtual stakeholder interviews, an interactive webmap, and in-person meetings

INITIATIVE RECOMMENDATIONS

Initiative recommendations were developed using the following outline:

a. Infrastructure Projects

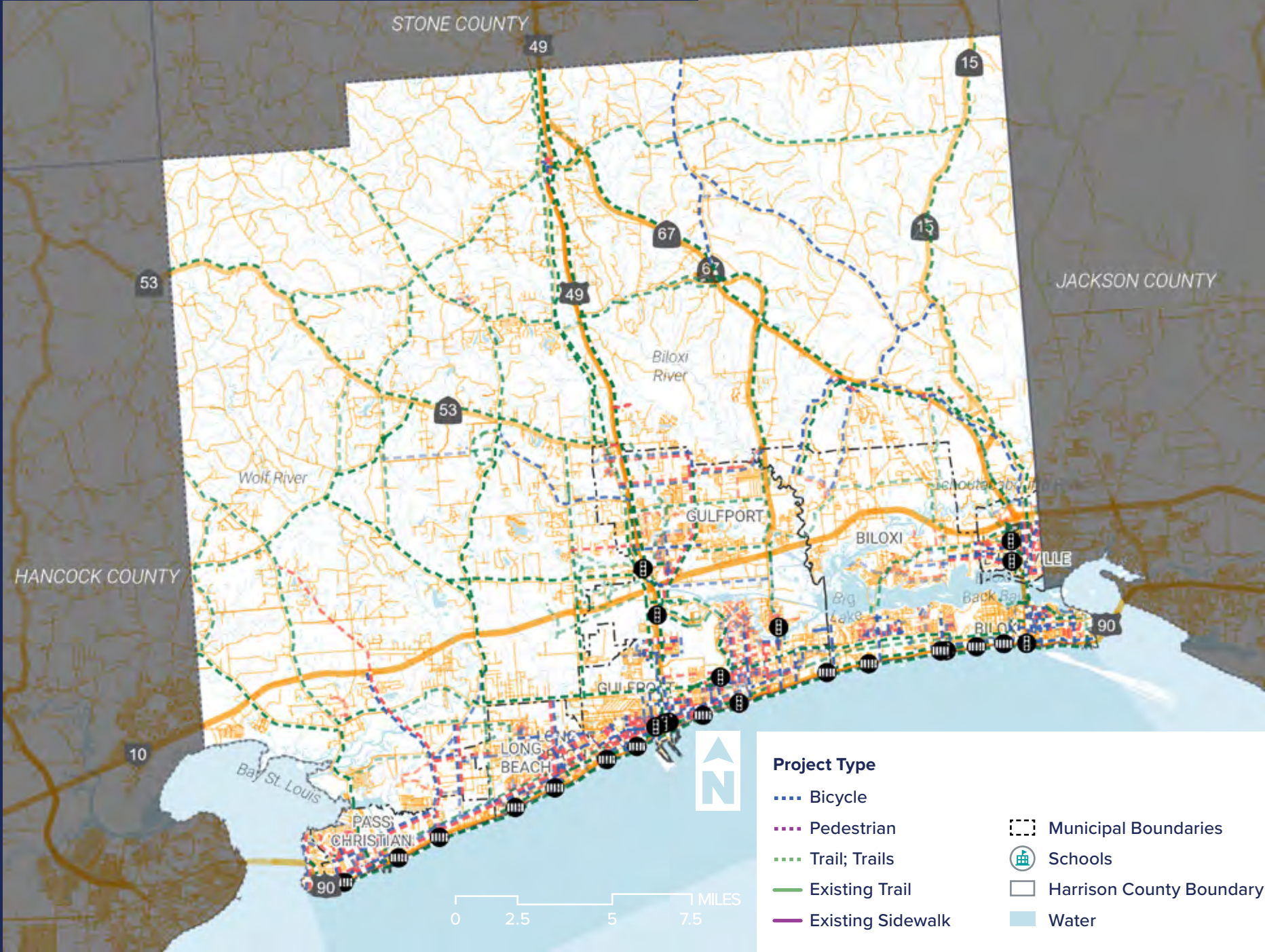
b. Policies

1. Local Development Ordinances
2. Bicycle Parking
3. Traffic Calming and Speed Reduction
4. Performance Metrics
5. Local Engineering Practices

c. Programs

1. Staff/Human Resources for Bicycle and Pedestrian Planning and Programs
2. Encouragement
3. Safety
4. Wayfinding System
5. Performance Metrics

RECOMMENDED ACTIVE LIVING NETWORK FOR HARRISON COUNTY





IMPLEMENTATION STRATEGIES

- 1 Maintain implementation responsibility** by assigning the duties of an active transportation planner to a staff person within the respective agency, and/or work with the Gulf Regional Planning Commission to create a full-time active transportation coordinator for the entire Mississippi Gulf Coast.
- 2 Strategically implement infrastructure projects and programmatic initiatives** by pursuing funding for high-priority projects and programs first, and by installing projects of all priorities in conjunction with larger construction projects.
- 3 Regularly update project prioritization** to ensure that new projects are added to the list, completed projects are removed, and the priorities are revised as conditions change.
- 4 Update the Initiative** on a regular basis. Current best practice is to update the bicycle portion of transportation planning strategies every five years. Other elements of the Initiative may be reviewed and updated as needed.
- 5 Coordinate with neighboring counties and cities** to create a safe, comfortable, and fun active transportation network across South Mississippi.

EARLY ACTION STEPS

- **Hire a Pedestrian & Bicycle Coordinator** to be housed at the County or Gulf Regional Planning Commission.
- **Review capital improvement and routine street maintenance projects** in the pipeline to determine if any of the recommendations in this Active Living Initiative can be added to the scope of work before implementation. In particular, consistently designed crosswalk striping should be included during any pavement rehabilitation.
- **Install “Bikes May Use Full Lane” signage** along popular rural bicycling routes throughout the County.
- Create a regionally consistent policy framework for **encouraging and later requiring Complete Street improvements in new development projects** (similar to Long Beach’s sidewalk policy).
- **Create a new pedestrian/bike/motor vehicle safety campaign** focused on communicating the rights of bicyclists and pedestrians to motor vehicle operators.
- Identify locations along Highway 90 to **install enhanced crosswalks, appropriate lighting, and beacons/signals** (e.g., Pedestrian Hybrid Beacons) at priority locations based on safety and access needs.







1

Community Vision

This chapter provides background information, a vision, and achievable goals for the Active Living Initiative.

Project Background

The Harrison County the Initiative develops a long-term vision as well as specific action steps for a more active, healthy, and livable county. The planning process involved in the the Initiative asked key stakeholders to envision what could encourage residents to be more active every day. This vision guides the specific network, policy, and program recommendations included in this report.

The Harrison County Active Living Committee began this study in 2020. Over the next two years, the Initiative was developed through interviews, field visits, remote analyses and research. Recommendations were made for unincorporated Harrison County and each of the incorporated municipalities (Biloxi, D'Iberville, Gulfport, Long Beach, and Pass Christian).

This Initiative will be useful to leadership in Harrison County, since it helps focus resources on the projects which will provide the most benefit. In addition, each project has been crafted as part of a long-term network, and therefore will be more competitive for state and federal funding.

VISION AND GOALS

The Initiative will guide Harrison County's investments in active living for the next twenty to thirty years. A specific vision for the future of the community helps shape the types of projects and policies that are developed for the Initiative, and inspires implementation of the Initiative after its adoption.

During the visioning process, community members decided which aspects of active living are appropriate for Harrison County. Questions that were considered included:



**WHAT KINDS OF TRAVEL
COULD BE ACTIVE?**

**WHO ARE THESE BICYCLE
AND WALKING PATHS SERVING?**

**WHAT ARE OUR LONG-TERM GOALS
AS A COMMUNITY FOR ACTIVE LIVING?**

VISION FOR THE HARRISON
COUNTY ACTIVE LIVING INITIATIVE

Harrison County will be a place in which incorporated and unincorporated areas are connected through a safe and engaging active transportation network that encourages a healthy, active lifestyle for residents and visitors alike.



The following goals were developed to guide the Active Living Initiative and its implementation.

1

Build out Harrison County's multimodal network. Plan, implement and maintain a comprehensive, connected, and efficient network of comfortable, safe and inviting sidewalks, bikeways, trails, and Complete Streets that link neighborhoods and activity centers, and provide convenient access to the widest range of destinations. Pedestrian and bicycle facilities will be designed to provide an accessible, low-stress experience for users of all ages and ability levels.

2

Reduce the frequency and severity of bicycle and pedestrian crashes through the implementation of systematic safety improvements and street designs that prioritize safety for vulnerable road users.

3

Incorporate practices that enhance equitable distribution of active transportation opportunities.



2

Existing Conditions

This chapter presents the current conditions of the infrastructure for bicycling and walking in Harrison County beginning with a review of current plans and policies. Analysis of crashes, identification of areas of equity concern, and destination access are also reviewed in this chapter.

Current Plans and Policies

The Active Living Initiative is consistent with and builds upon the efforts of various local planning and policy documents . Harrison County also intends to design an active living network that continues to complement existing and planned bikeways and pedestrian projects in surrounding communities. Therefore, the planning context also includes bicycle and pedestrian plans, policies, and projects in surrounding counties. The following table lists some of these relevant documents, and a detailed review can be found in Appendix A.

PLAN	JURISDICTION	DATE ADOPTED
Multiplan 2045: Mississippi's Unified Long-Range Transportation Infrastructure Plan	Mississippi Department of Transportation (MDOT)	2020
2045 Metropolitan Transportation Plan for the Mississippi Gulf Coast Metropolitan Planning Organization	Gulf Regional Planning Commission (GRPC)	2020
D'Iberville Sidewalk and Pedestrian Study	City of D'Iberville	2020
Pedestrian Study for the US Highway 49 Corridor	City of Gulfport	2020
Downtown Gulfport Master Plan	Gulfport Main Street Association (GMSA)	2018
Gulfport Pedestrian Sidewalk Master Plan	City of Gulfport	2015
Community Plan for Western Harrison County	Harrison County	2008
Community Plan for DeLisle	Harrison County	2006

Existing Active Transportation Network

There are currently limited amounts of dedicated bicycle and pedestrian facilities in Harrison County. Multi-use paths/trails and on-street bike lanes are segmented, with only a few projects in place, and many of the designated “bike routes” in the cities and unincorporated communities consist only of signs and/or notation on maps. Some portions of the incorporated cities’ commercial districts have sidewalks, often connecting to Highway 90/Beach Boulevard or schools, while the county’s unincorporated areas primarily lack dedicated spaces for walking.

Pass Christian, for example, has sidewalks along Second Street, Market Street, and Davis Avenue leading to the harbor. **The existing environment along Second Street is welcoming, with wayfinding and placemaking elements already established.** Long Beach already has sidewalks which seem to be tied to the schools in the city. Sidewalks are present between Long Beach Middle and High Schools, as well as near W. J. Quarles Elementary and Harper McCaughan Elementary. Neighborhoods near Thomas L. Reeves Elementary also already have sidewalk facilities.

Gulfport pedestrian facilities are concentrated between 42nd Avenue and 25th Avenue primarily near Beach Boulevard. The sidewalk network currently extends perpendicular from Beach Boulevard all the way to 28th Street along 33rd Avenue; 30th Avenue; 25th Avenue; Courthouse Road; Cowan Road; Anniston Avenue; and Debuys Road. **Similar to Long Beach, the sidewalks in Gulfport are centered around the commercial businesses**

along the beach and the harbor, however there are some facilities in the northern portion of the city, namely along Dedeaux Road, Klein Road, Crossroads Parkway, and Seaway Road.

Similar to Gulfport’s, **Biloxi pedestrian facilities are located along many of the primary roadways perpendicular to the coast** including: Beauvoir Road, Veteran’s Avenue, Rodenberg Avenue, Iberville Drive, St. Charles Avenue, White Avenue, Porter Avenue, Reynoir Street, Main Street, Oak Street, and more. Similar to other cities, a large number of sidewalks exist in the main commercial district near Main Street and the coast. Of note, Atkinson Road, Jim Money Road, Jam Lane, and Richard Drive already have sidewalks, although not immediately adjacent to the coast.

Atkinson Road and Jim Money Road, in particular, connect to the existing multi-use path leading into Hiller Park in Biloxi. Another multi-use path connects Biloxi to D’Iberville along the Interstate 110 bridge over the Back Bay. In addition, there are existing sidewalks on Central Avenue, Promenade Parkway, and Sangani Boulevard in D’Iberville.

Some of the most distinctive multi-use paths in Harrison County are those connecting the coastline along bay bridges. The Bay St. Louis Bridge connecting to Pass Christian has a barrier-separated multi-use path. The Biloxi Bay Bridge also incorporates a barrier-separated multi-use path from Biloxi to Ocean Springs. Both of these bridges allow for a unique experience riding or walking surrounded by the ocean and sky.

Figure 1 Existing Network for Walking and Bicycling

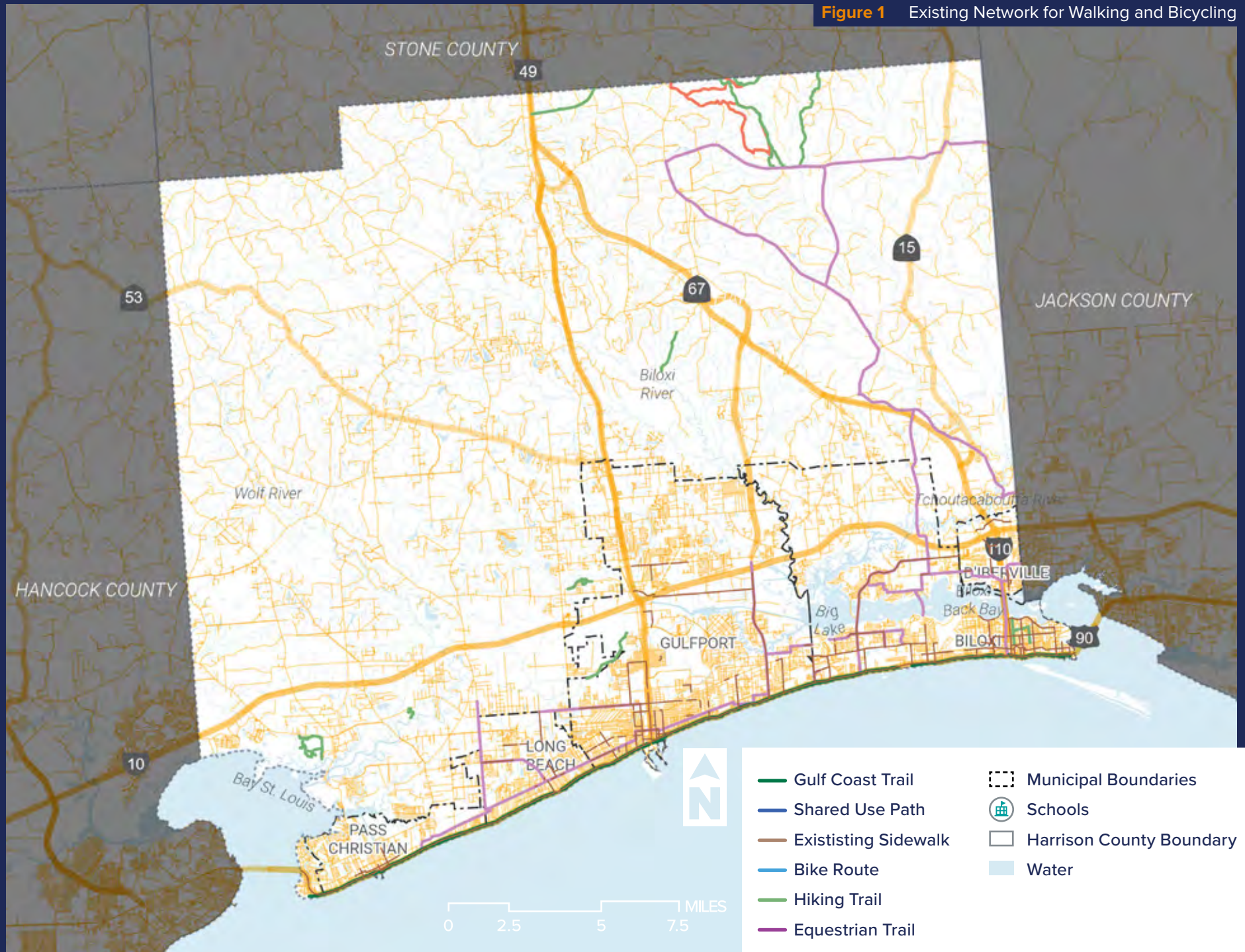


Figure 2 Existing Network: Pass Christian and Long Beach

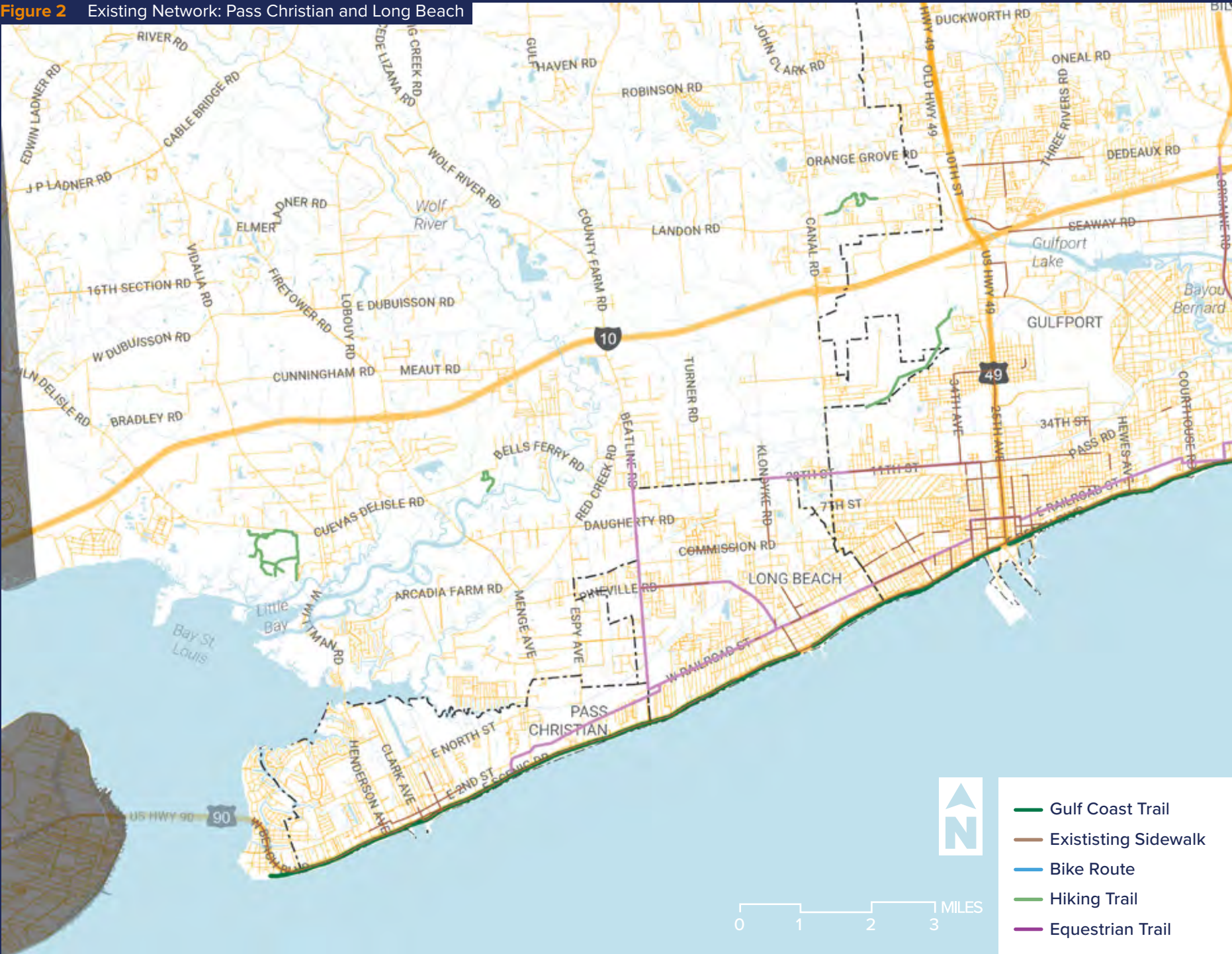
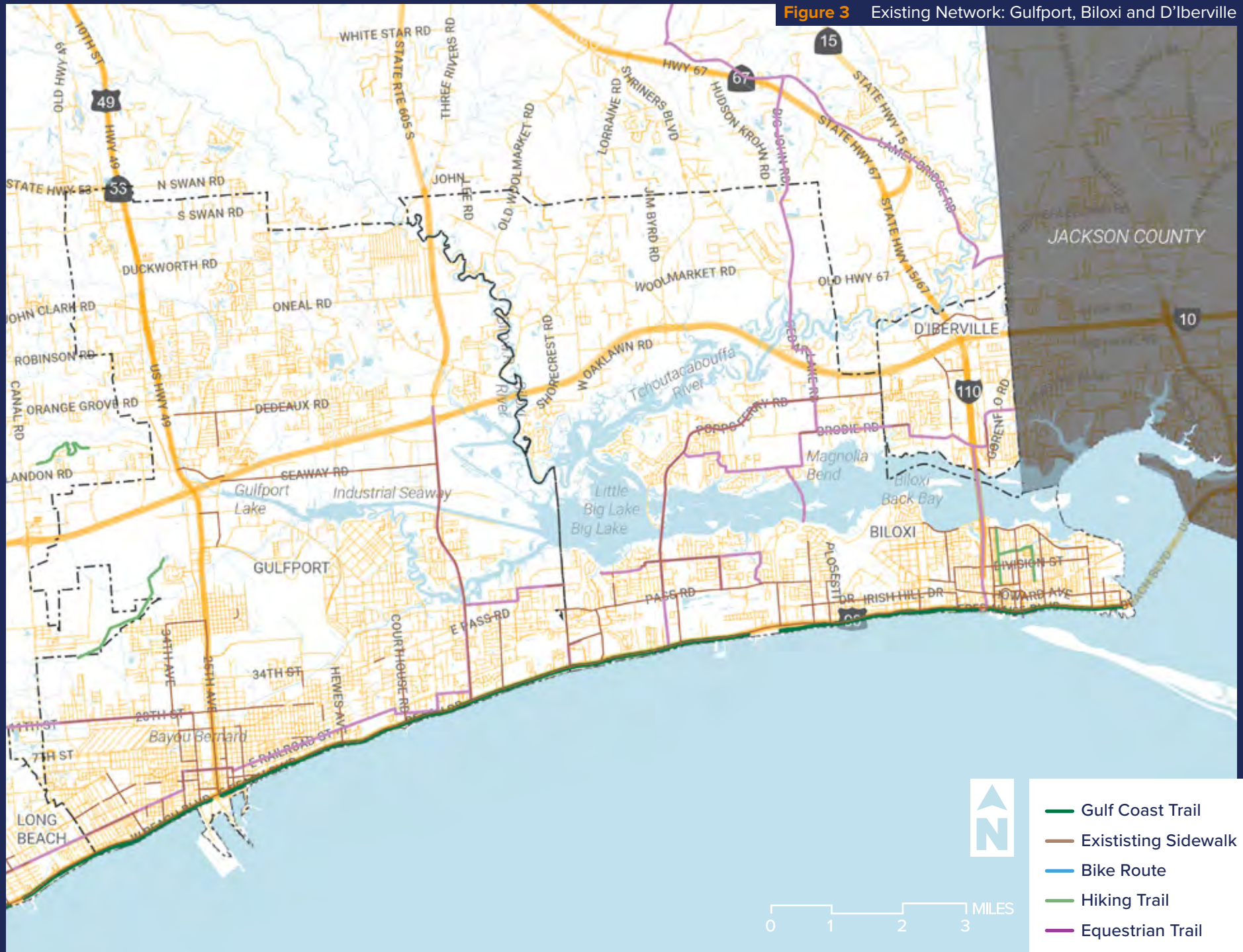


Figure 3 Existing Network: Gulfport, Biloxi and D'Iberville



Equity Analysis + Employment Centers

PROCESS

The equity analysis conducted as part of this Active Living Initiative considered demographic factors that indicate locations in Harrison County where there are concentrations of vulnerable populations. These areas of concern were considered in project development and were included as a prioritization model input for recommended projects. **Active transportation investments in these areas are likely to improve access to jobs, education, healthcare, and other important destinations.**

The equity analysis used a combination of six socioeconomic characteristics as indicators to identify populations that are vulnerable to unsafe, disconnected, or incomplete active transportation facilities. The map on the facing page displays a composite map that combines all of these factors for an overall Equity score.

The indicators include:

- **Age:** Individuals under the age of 18 and over the age of 65 comprise this indicator.
- **Race:** This indicator measures the percentage of the population that identifies as non-white.
- **Public Assistance:** This indicator measures the percentage of households receiving public assistance in the past 12 months.
- **Educational Attainment:** This indicator represents the percentage of the population over 25 years of age that does

not have a high school diploma or equivalent.

- **Limited English Proficiency (LEP):** This indicator measures the percentage of the population that identifies as not speaking English well or at all.
- **Disability:** This indicator measures the percentage of the population that is disabled.

The economic analysis identified key employment centers for the Harrison County ALP. These centers were located in order to create and prioritize projects that would connect to existing areas of employment.

KEY FINDINGS

The equity analysis indicates that vulnerable populations tend to be located near I-10 and US-49, mostly in the southeast portions of the county near the coast. The development of the walking and bicycling network should consider that the people who live in these areas may be more likely to walk or bicycle out of necessity, rather than by choice.

Employment centers are located near I-10 at Kiln Delisle Rd and US-49; downtown Gulfport; along I-110 near the Biloxi Back Bay bridge; and along US-90 east of I-110 in Biloxi. Project recommendations were prioritized with these areas in mind.

Figure 4 Equity Analysis

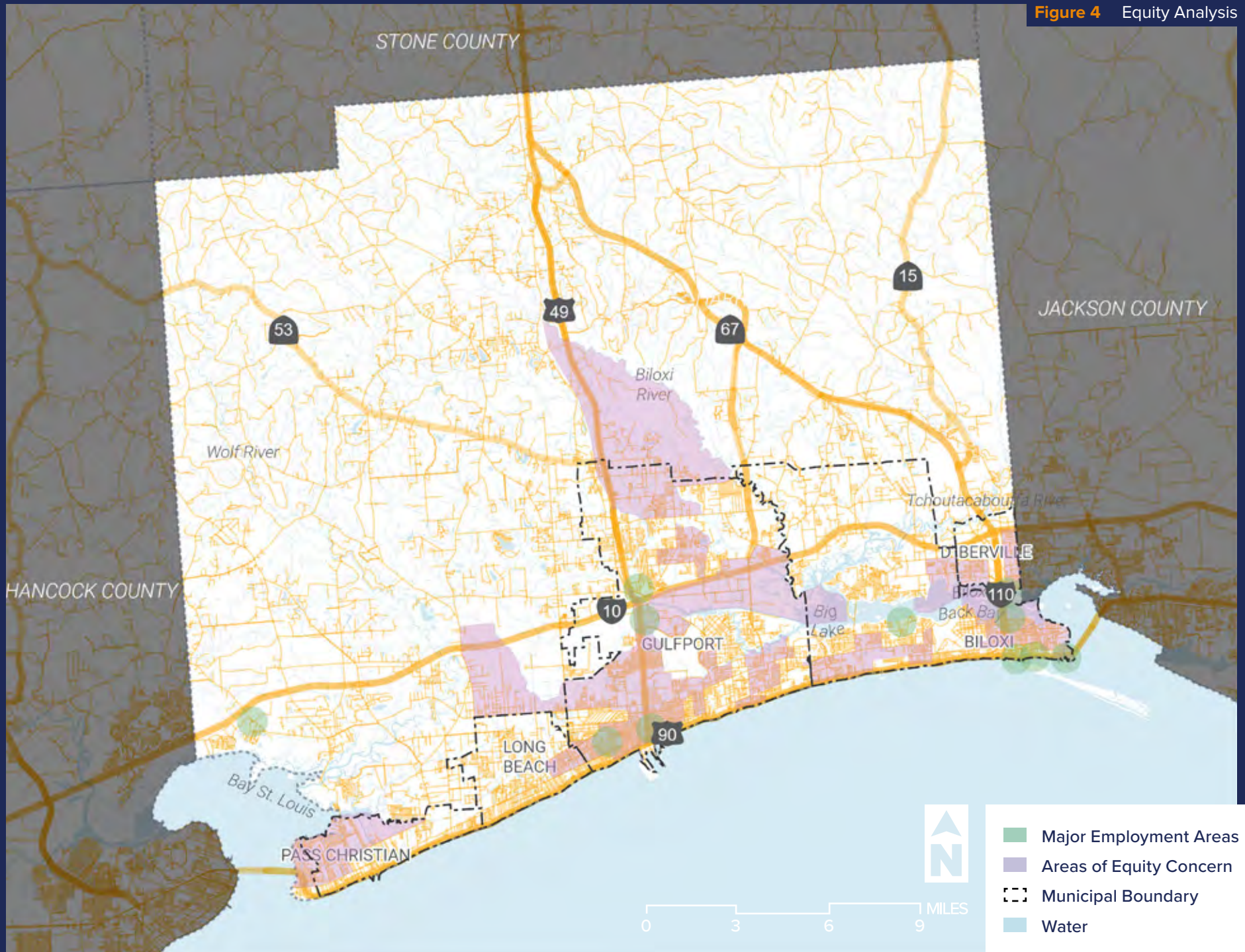


Figure 5 Equity: Pass Christian and Long Beach

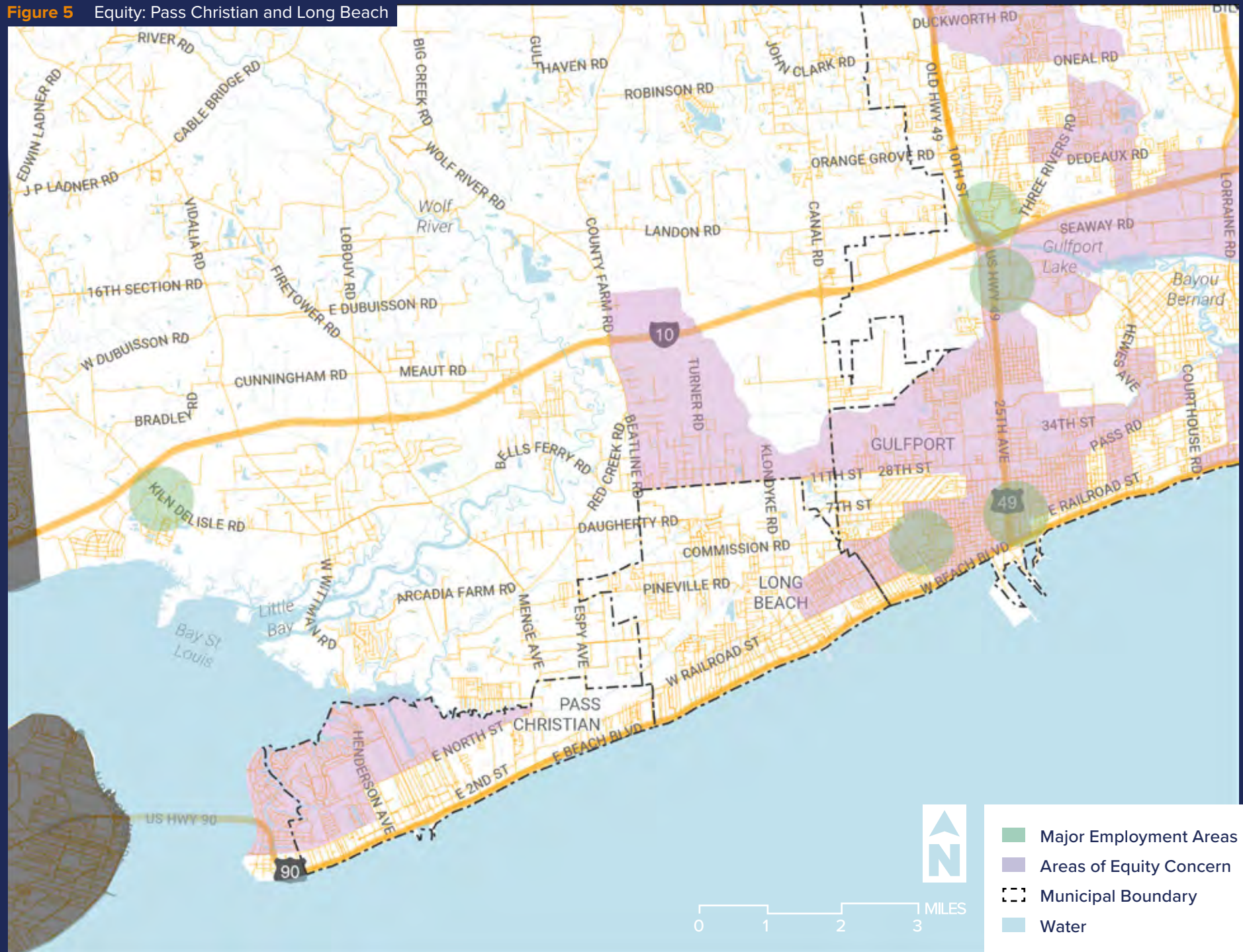
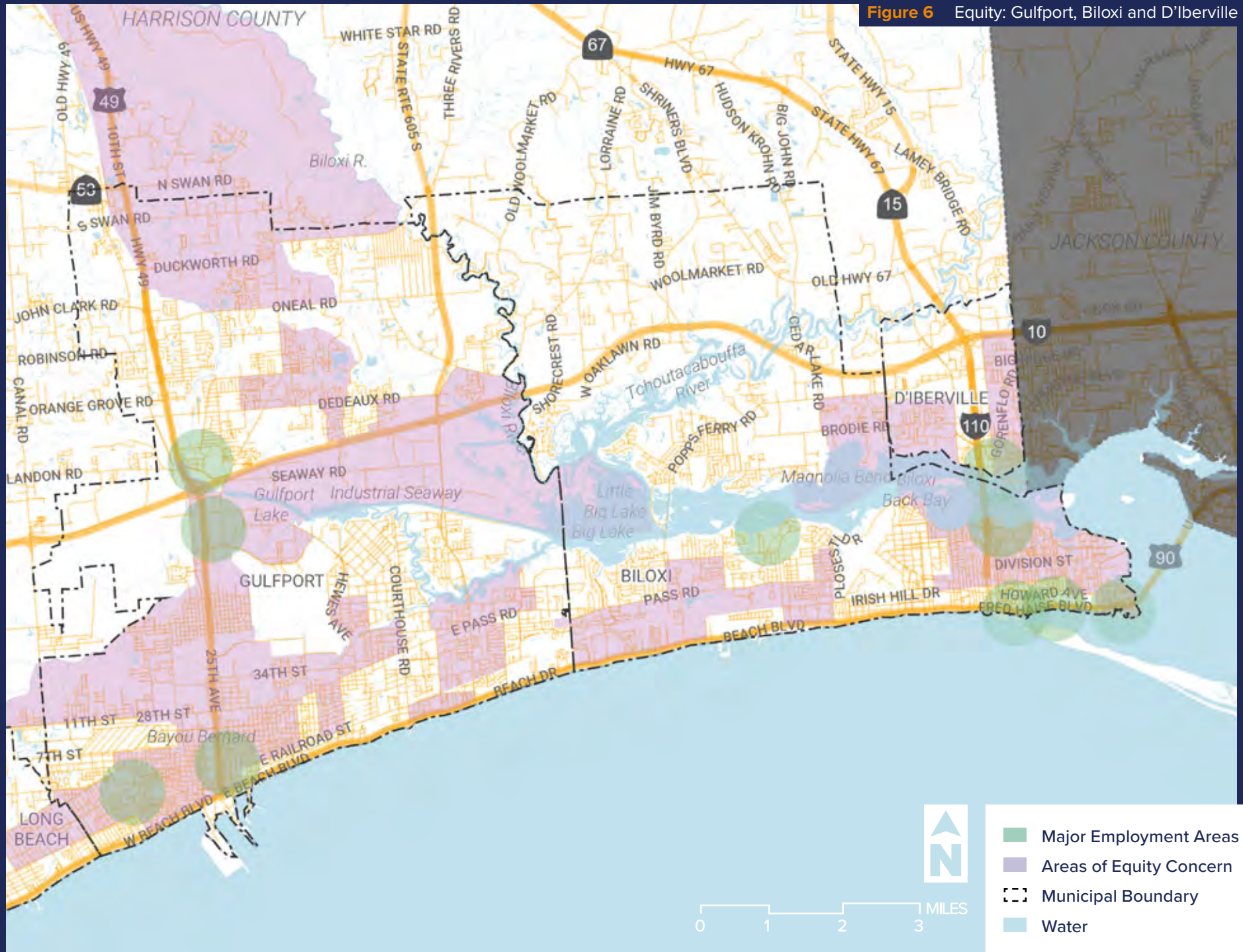


Figure 6 Equity: Gulfport, Biloxi and D'Iberville



Safety Analysis

Between 2016-2020, approximately 20 people each year lost their lives while riding bicycles or walking in Harrison County. In addition, an average of 150 people are seriously injured each year while walking or biking across Harrison County.

Over the period reviewed, there were:

- **94 fatalities** suffered by a bicyclist or pedestrian
- **447 injuries** suffered by a bicyclist or pedestrian

Light conditions are particularly important in the case of fatal crashes, as 61 percent of all fatalities for those walking and

bicycling occurred in dark conditions. Even in lighted areas, careful consideration for pedestrian and bicyclist scale lighting is important, as an additional 25 percent of all fatalities occurred in dark-lighted conditions (that is, during nighttime hours where lighting was available). The remaining 14 percent of fatalities occurred in dusk or daylight conditions.

Of those reporting (68 percent of all bicycle and pedestrian crashes), only 10 percent of pedestrian and bicyclist crashes involved a person under-the-influence of alcohol.

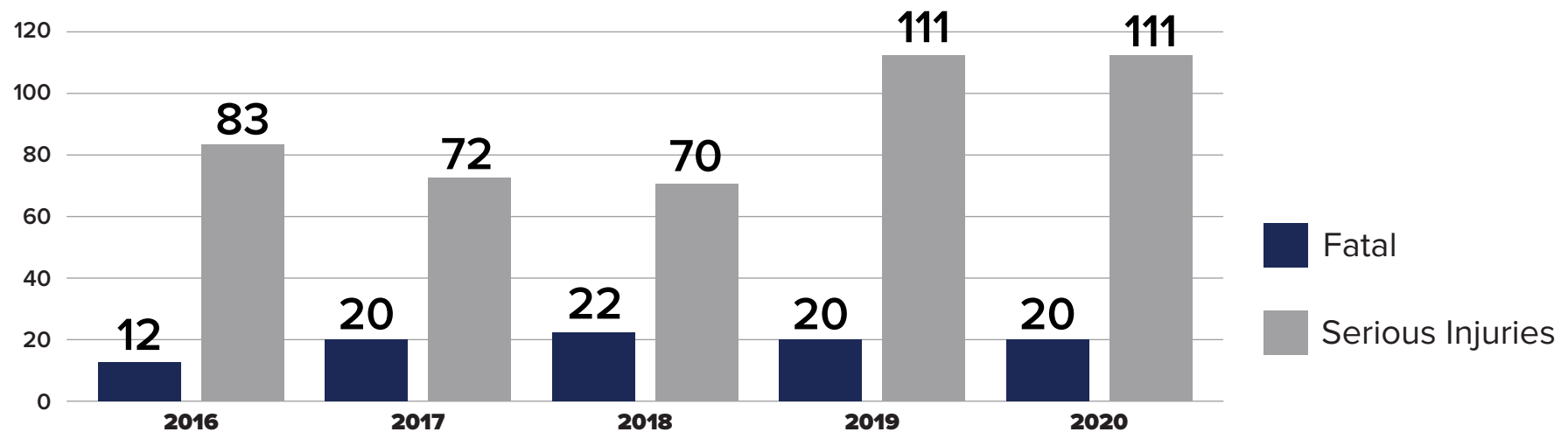


Figure 7 Crashes Involving People Walking and Bicycling that Resulted in Fatalities or Serious Injuries (2016-2020)

Figure 8 Crashes Involving Pedestrians & Bicyclists (2016-2020)

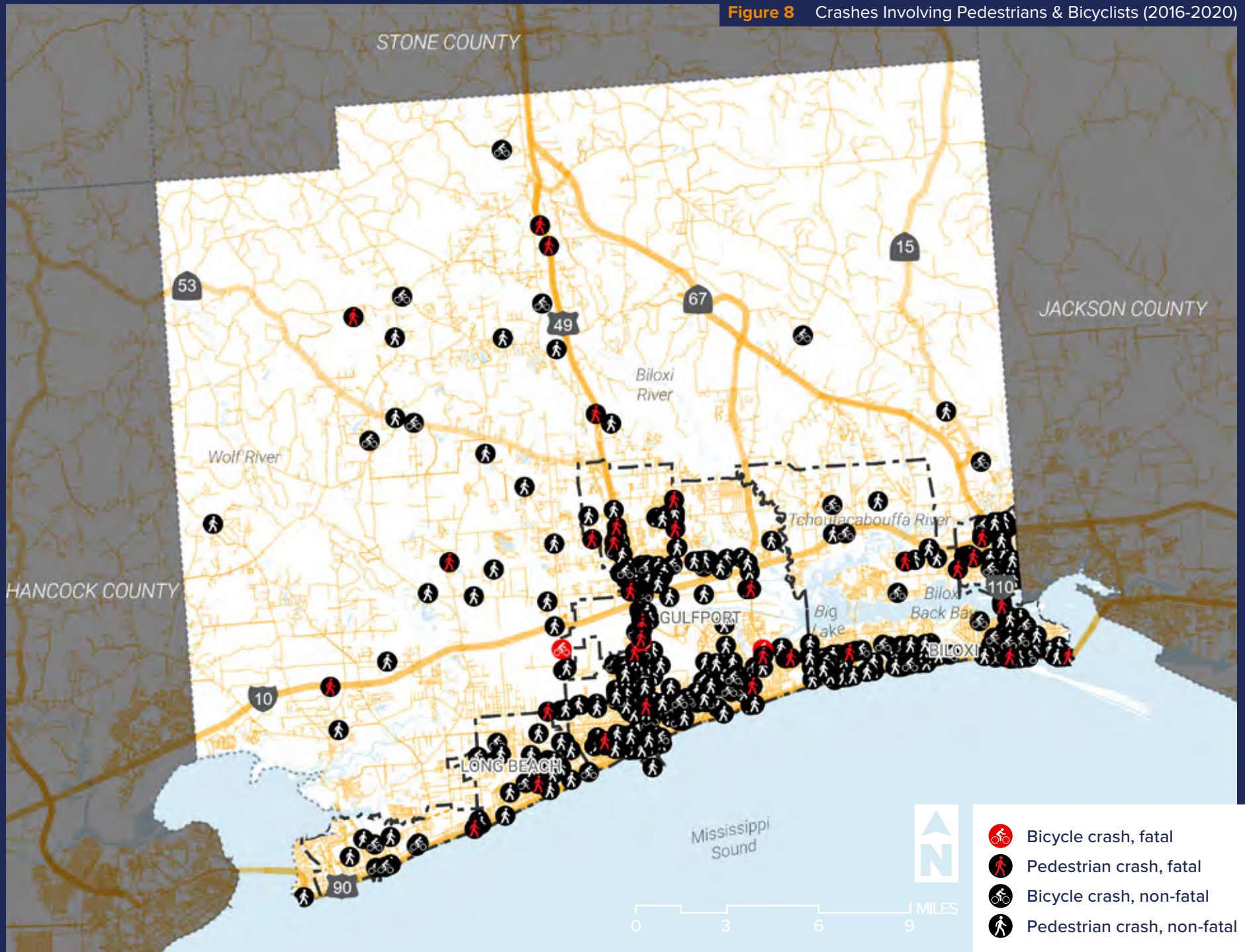


Figure 9 Crashes: Pass Christian and Long Beach

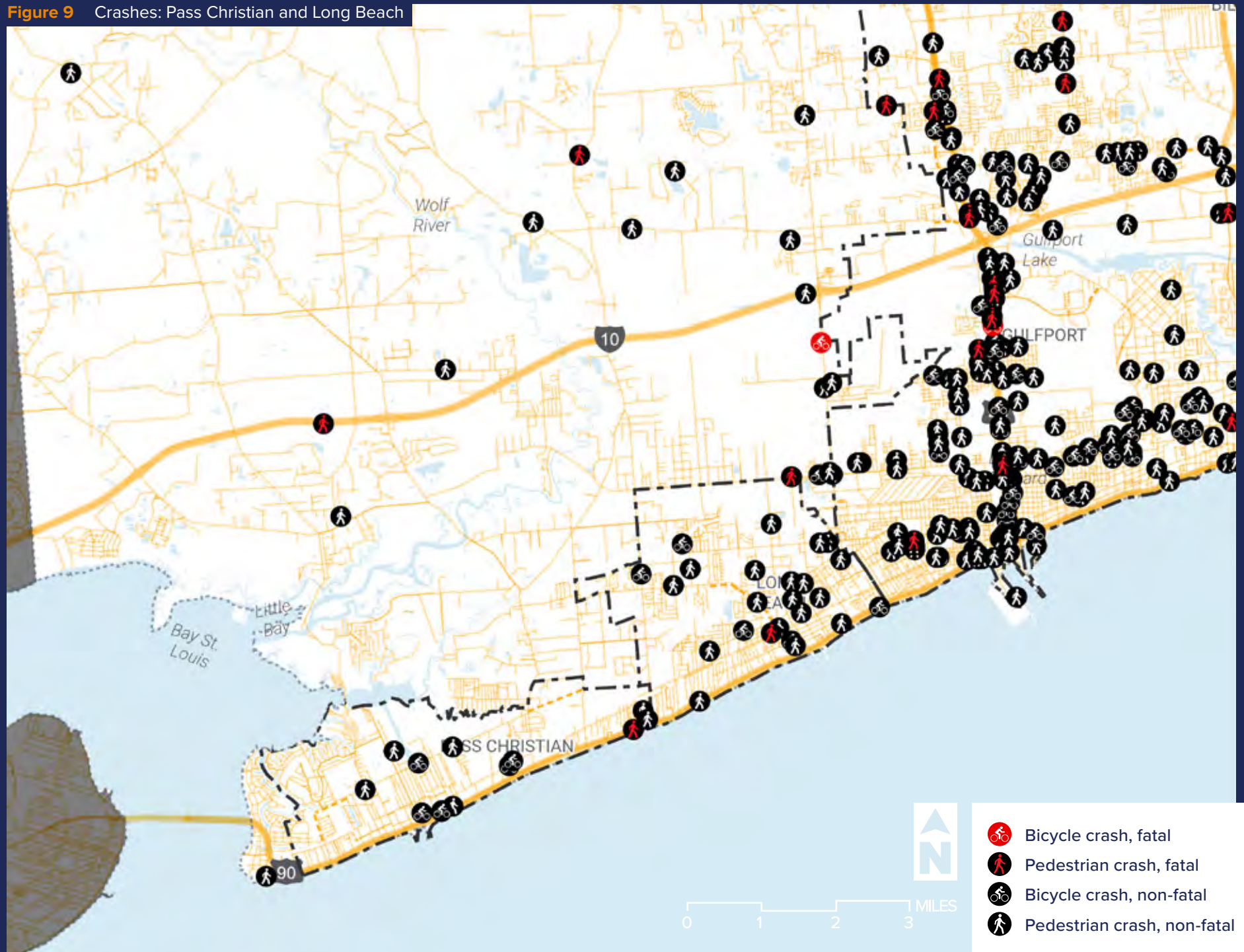
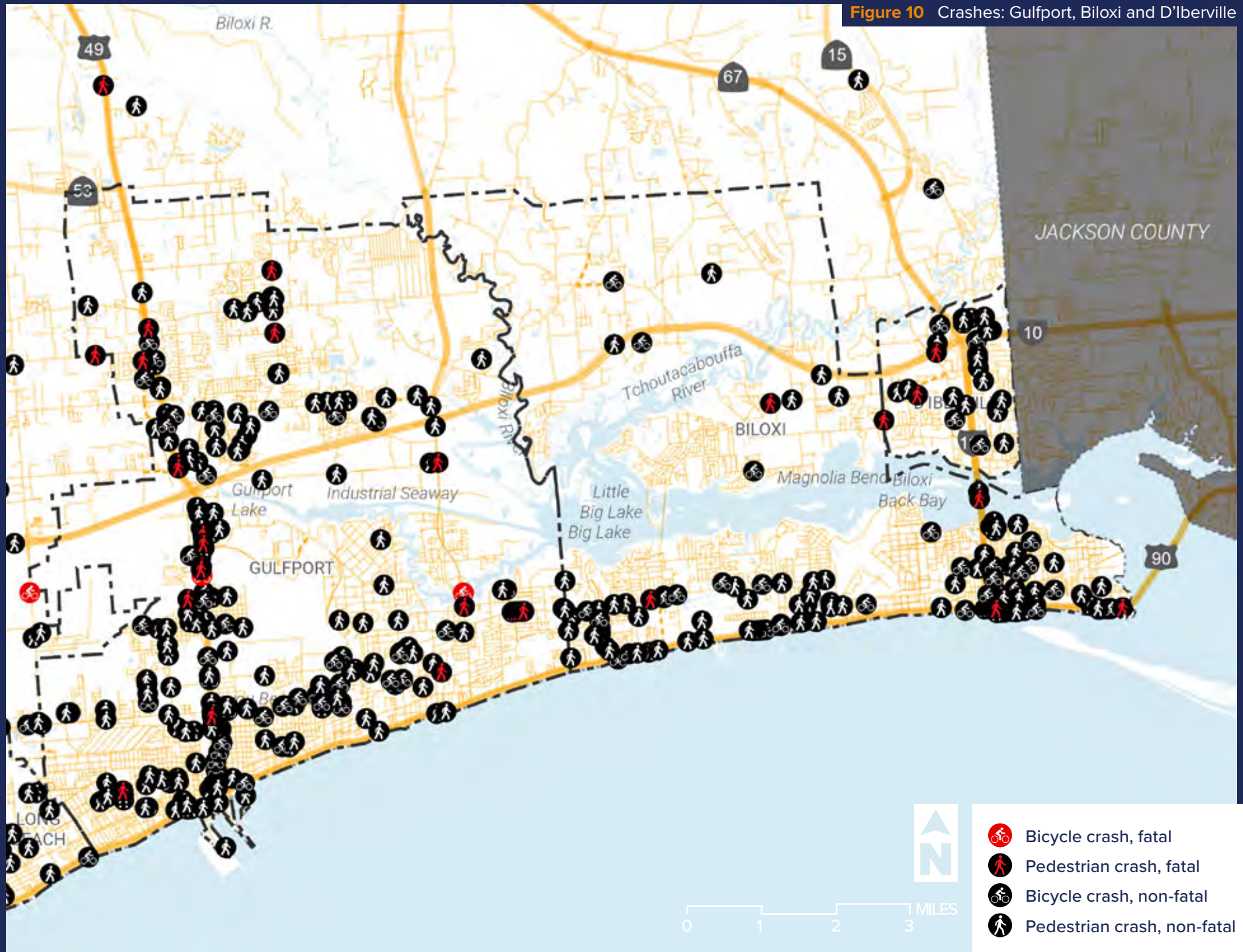


Figure 10 Crashes: Gulfport, Biloxi and D'Iberville



Access to Key Destinations

PROCESS

The proximity to schools and city centers was identified for the Harrison County Active Living Initiative. The 15 minute network walksheds from each school and downtown commercial district were mapped in order to identify and prioritize projects that would connect to important community destinations.

As shown in the following maps, many key destinations for Harrison County are located near the coast. Many of these walksheds are

larger than those located further inland. Shorter blocks, and grid-like street network that is more common in the cities allows for larger walksheds around key destinations. The road network is more limited inland, and so the walksheds tend to be more linear in unincorporated Harrison County.

The Initiative prioritizes projects using these key destination walksheds as one input determining the overall access score for the project.



Figure 11 Access to Key Destinations

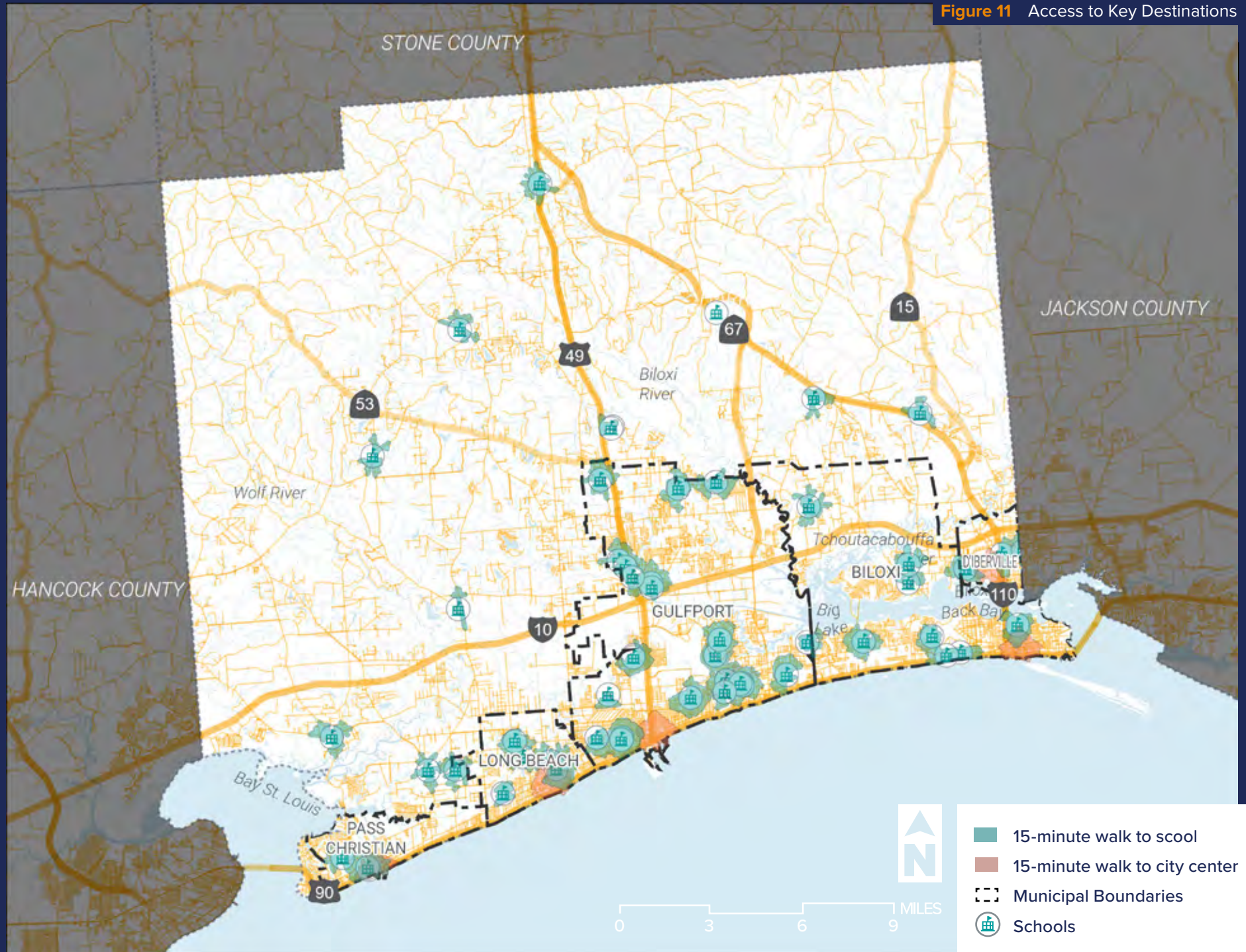


Figure 12 Key Destinations: Pass Christian and Long Beach

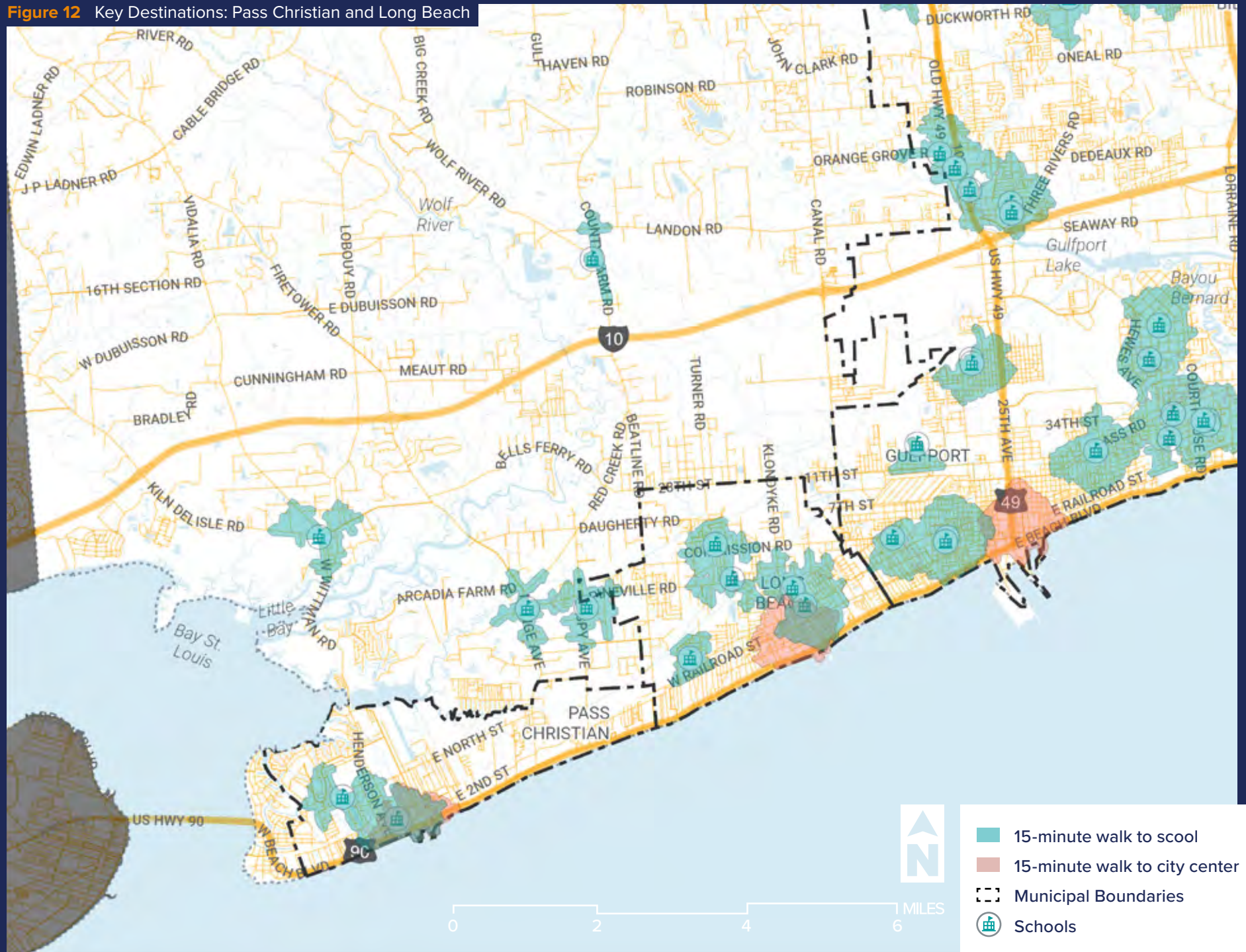
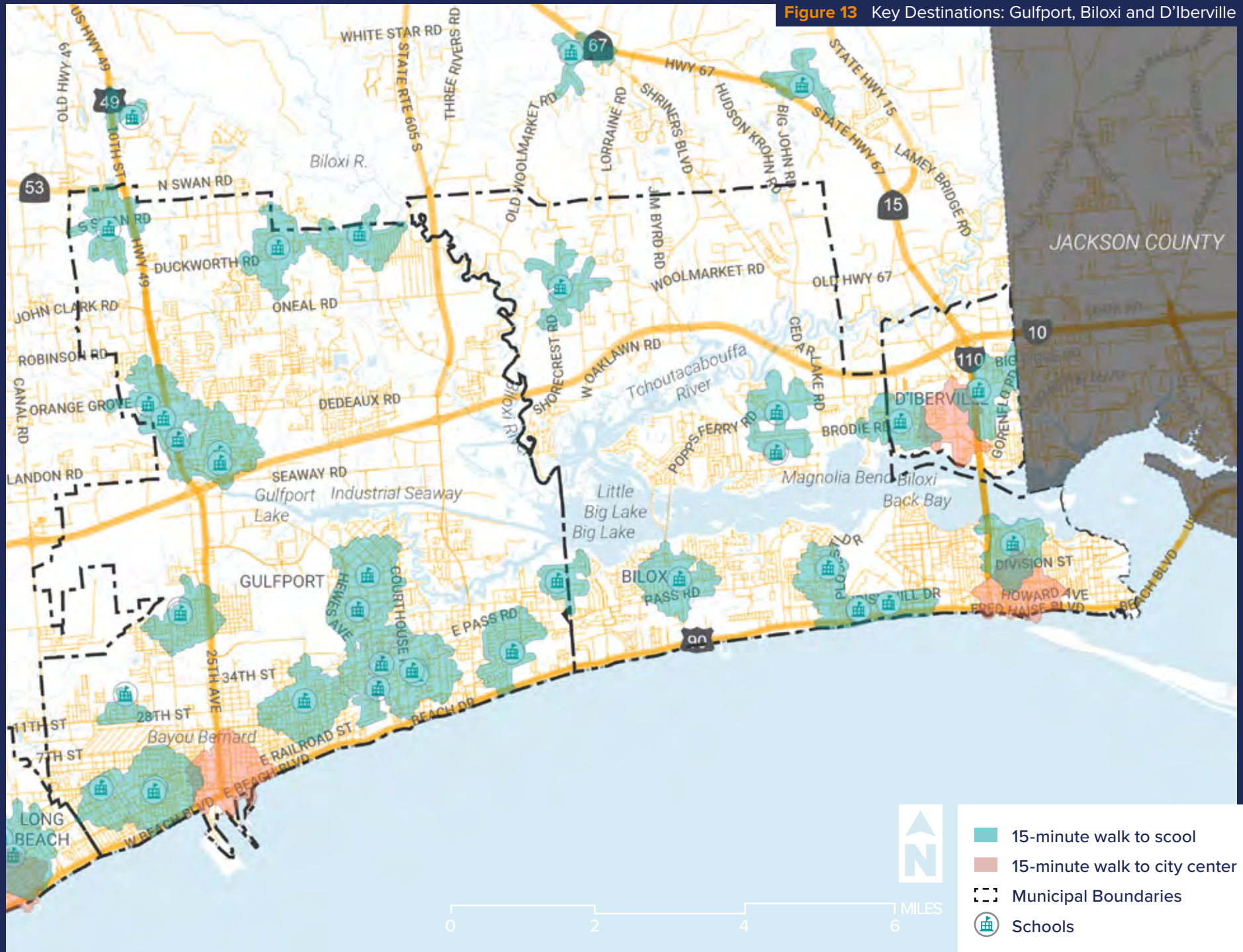


Figure 13 Key Destinations: Gulfport, Biloxi and D'Iberville



Community Engagement

In the past, city designers crafted plans for communities that were limited to their own imaginations. Today, plans are developed through an iterative process that includes research, analysis, as well as the knowledge and preferences of community residents.

Key barriers to walking and bicycling like busy roads, limited sidewalks, or particular land uses were identified. Opportunities like the beaches and parks, neighborhoods with lots of families, or additional partnerships were also discussed. These conversations helped build the vision for the Initiative, and also shaped recommendations throughout.





Comment Card		<i>Harrison County Active Living Initiative</i>  
<input type="checkbox"/> <i>I prefer to remain anonymous.</i>		
Name/Email (optional): _____		
Phone # (optional): _____		Zip Code: _____
Comment: _____ _____ _____ _____ _____ _____ _____		
<i>In a hurry? You can also email your comments to ryanjohnson@altago.com</i>		
Tarjeta de Comentarios		<i>La Iniciativa de Vida Activa para el Condado de Harrison</i>  
<input type="checkbox"/> <i>Prefiero permanecer anónimo.</i>		
Nombre/Correo Electrónico (opcional): _____		
Teléfono # (opcional): _____		Código postal: _____
Comentario: _____ _____ _____ _____ _____ _____ _____		
<i>¿Tienes prisa? Puede enviar un correo electrónico con tus comentarios a ryanjohnson@altago.com</i>		

Figure 14 Bilingual Comment Card

STAKEHOLDER INTERVIEWS

The community engagement process began with interviews of small groups of key community stakeholders. The project team conducted more than a dozen of these stakeholder interviews in a virtual format from November 2019 to winter 2022.

In addition to the interviews, in-person feedback was obtained during a bike ride on April 30th, 2021, as well as visits to the community in Winter 2021.

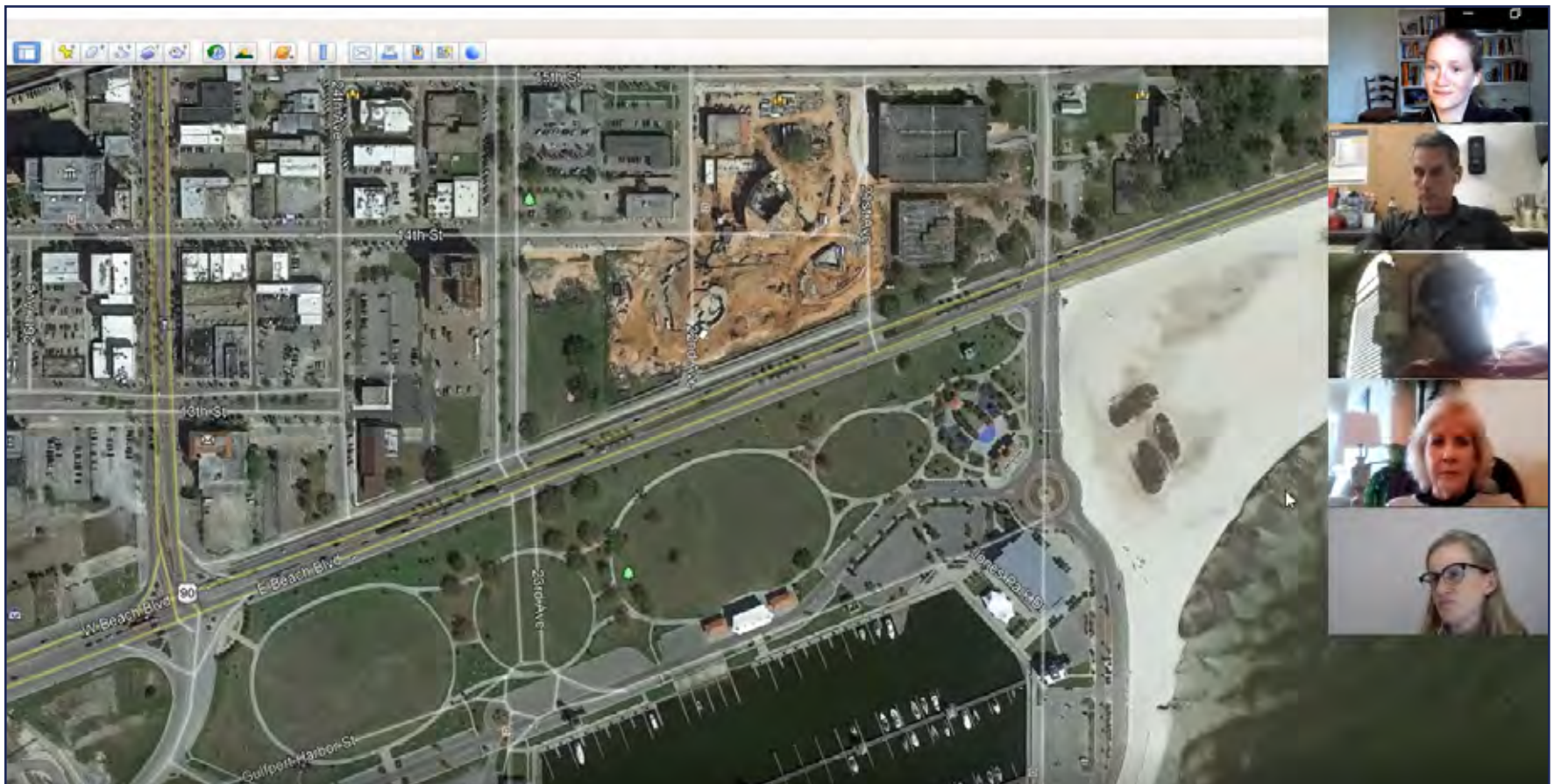


Figure 15 Stakeholder Interviews were held virtually

WEBMAP COMMENTS

Typically, in-person workshops are used to collect general feedback on community plans. However, this project was affected by an unprecedented global pandemic as well as Hurricane Ida in October 2021. The project team moved quickly to develop a webmap, and allow virtual public feedback.

Harrison County residents responded to the webmap with more than 700 comments, votes, and suggestions. These suggestions helped form the project vision and goals, critiqued draft recommendations, and inspired policy recommendations.

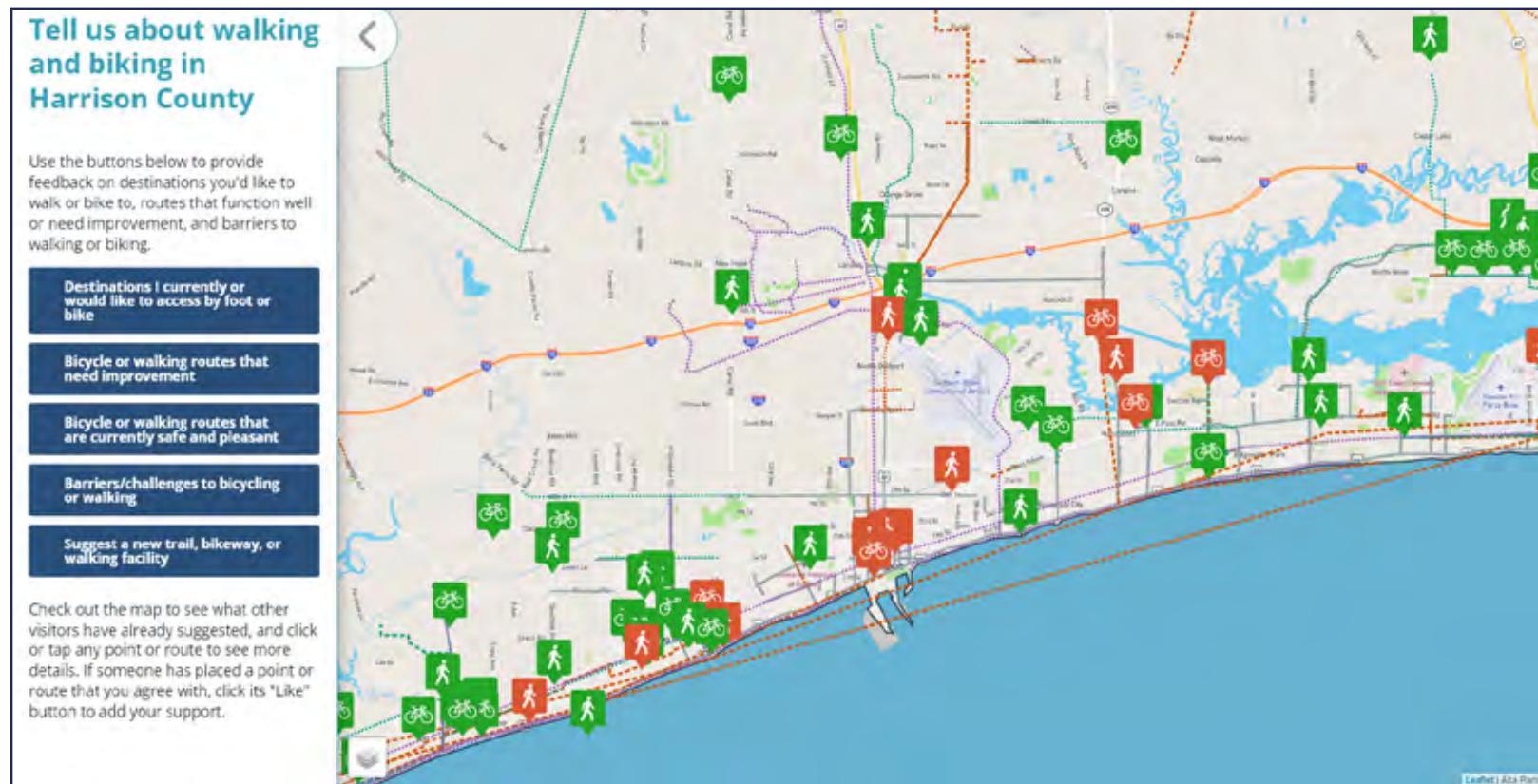


Figure 16 Public Input Webmap

Each specific comment or suggestion could be voted on with a “like” or “dislike” button. Additional comments could also be made by the public to each individual point on the map.

The public webmap was available from June 2021, to February 2022. Feedback from the webmap was utilized to draft and refine the initiative’s recommendations.

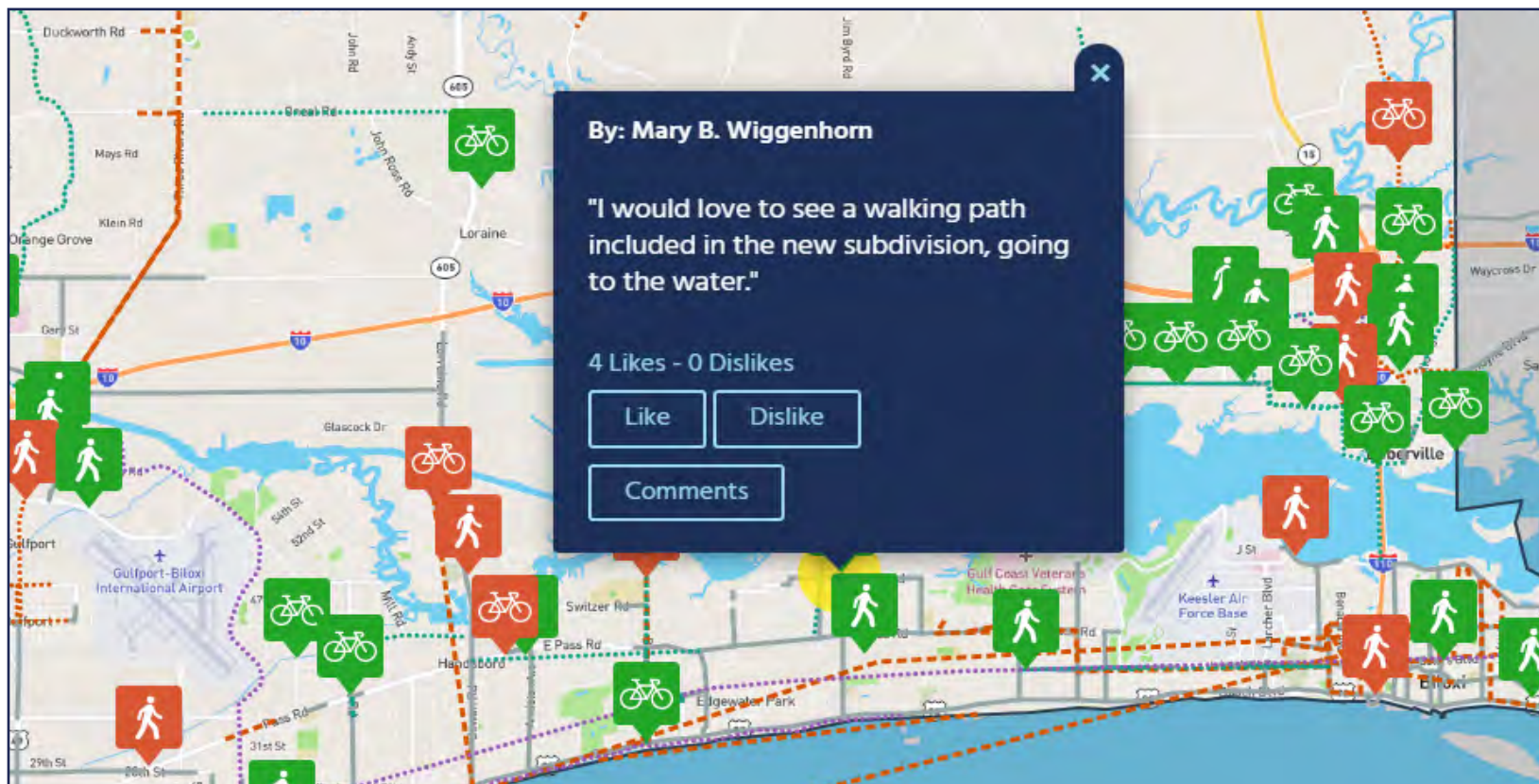


Figure 17 Public Input Webmap Detail

STAFF AND COMMITTEE MEETINGS

Over the course of the project, in-person meetings and interviews were held strategically with the Active Living Committee, County staff, and representatives from the cities. Educational materials focused on walking and bicycling infrastructure were developed for the sessions. The project team also developed a visual preference survey. Meeting attendees voted on the types of bicycling and walking improvements they would like to see in Harrison County.

These fruitful discussions led to the specific recommendations outlined in the following chapter.





Figure 18 Visual Preference Survey used during in-person meetings

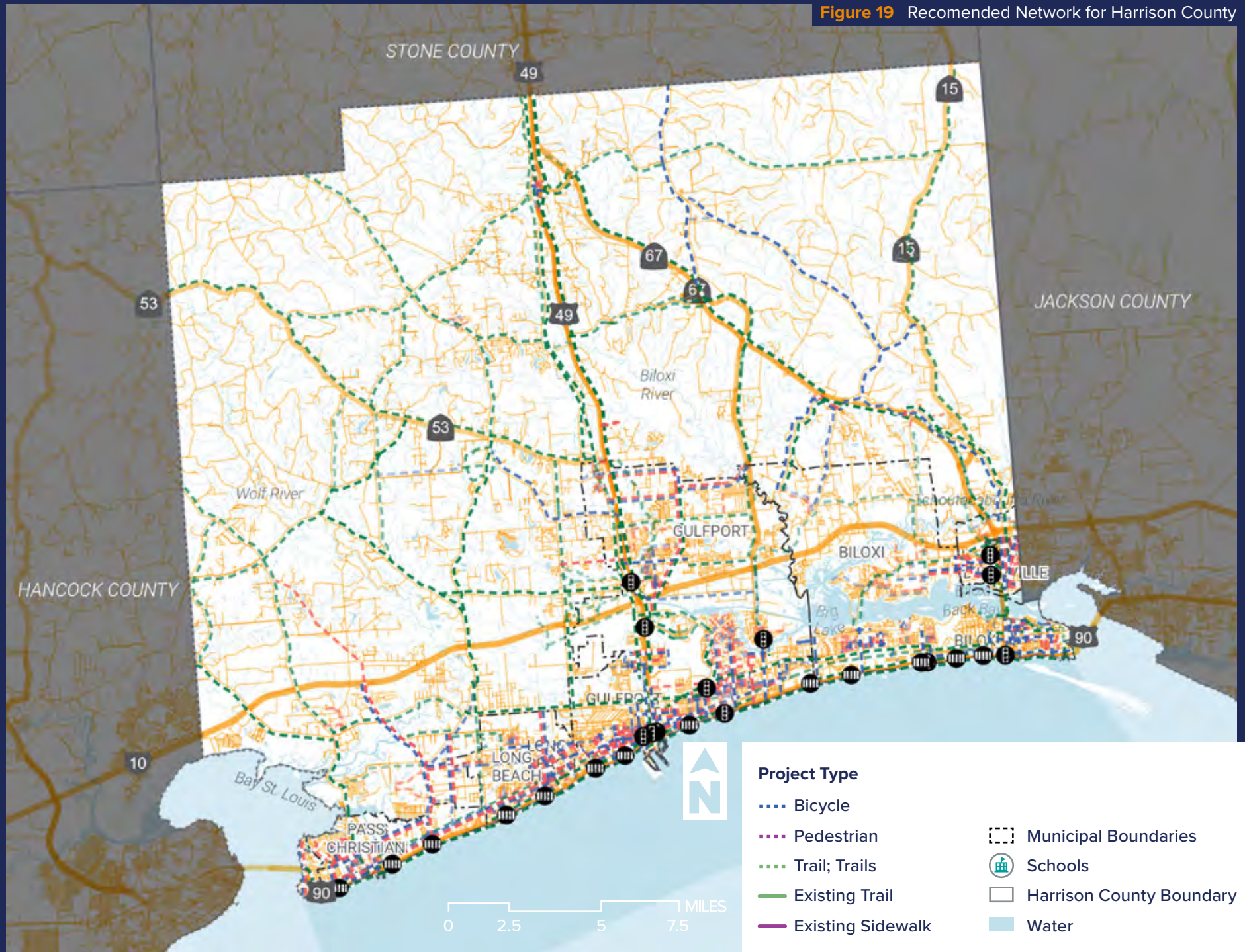


3

Recommendations

This chapter describes a selection of potential pedestrian and bicycle facility types, followed by a comprehensive set of infrastructure recommendations which Harrison County and its cities will use to guide the development of its active transportation network. Chapter 3 concludes with supportive policy and program recommendations.

Figure 19 Recommended Network for Harrison County



Bikeway Facility Types



SIGNED BIKE ROUTE

A signed bike route is a low-cost, easy to implement method for encouraging cycling on low-volume residential streets, and for filling in the gaps in the bikeway network.



ENHANCED SHARED ROADWAY

There may not always be an opportunity to create a dedicated bikeway or off-street path. In these cases, a marked shared roadway (shared lane markings + “Bike Route” signage) can be enhanced with bicycle-oriented wayfinding and selected traffic calming devices.



NEIGHBORHOOD GREENWAY/BIKE BOULEVARD

A neighborhood greenway or bicycle boulevard is a low-stress shared roadway that is designed to offer priority for bicyclists operating within a roadway shared with motor vehicle traffic. facilities include traffic calming elements such as speed humps, chicanes, and traffic circles as well as lower speed limits, wayfinding signage, and shared lane markings.



BIKE LANES

Bike lanes designate an exclusive space for bicyclists through the use of pavement markings and signage. Bike lanes make bicycling a more visible and comfortable option for people who usually would drive or walk.



BUFFERED BIKE LANES

Buffered bike lanes are conventional bike lanes (see “Bike Lanes” definition above) with a painted buffer between the bike lane and the travel lane. Buffered bike lanes provide added safety and comfort by further separating bicyclists from motorists.



SEPARATED BIKE LANES

Separated Bike Lanes, sometimes called “Cycle Tracks,” or “Protected Bike Lanes” are dedicated bikeways that use a vertical element to provide separation from motor vehicle traffic. The vertical separation discourages drivers from parking or idling in the bikeway.



PAVED SHOULDER

Paved shoulders on the edge of roadways can be enhanced to serve as a functional space for bicyclists to travel in the absence of other facilities with more separation.



MULTI-USE PATH/TRAIL

A multi-use path provides a travel area separate from motorized traffic for all users. Multi-use paths are desirable for bicyclists of all skill levels preferring separation from traffic. Multi-use paths may also be implemented immediately adjacent and parallel to a roadway, or in their own independent right-of-way.

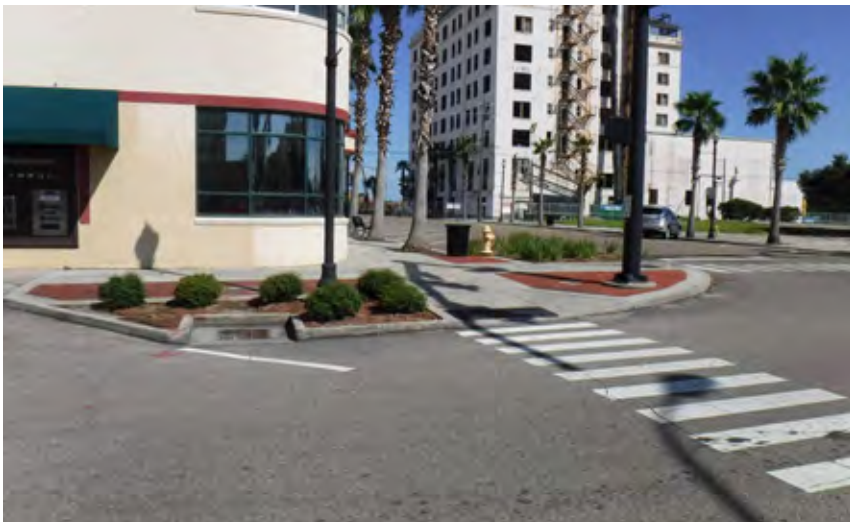
On some roadways where different user types are anticipated, multi-use paths are recommended along with an on-street bike facility.

Pedestrian Facility Types



HIGH-VISIBILITY CROSSWALKS

High-visibility crosswalks include additional paint that can enhance a motorist's awareness of a crosswalk. Near schools, crosswalks are painted yellow for additional visibility. In-roadway lighting can further enhance crosswalk visibility.



CURB EXTENSIONS & MEDIAN REFUGE AREAS

These treatments reduce pedestrian crossing distances and improve visibility. Curb Extensions also reduce vehicle speeds by reducing turning radius, which increases the chance of survival for a pedestrian in the event of a collision. The added pedestrian space created by curb extensions can also be used for landscaping.



RECTANGULAR RAPID FLASH BEACONS

Rectangular Rapid Flash Beacons enhance the visibility of crosswalks marked by just paint. Flashing lights and additional signage alert motorists to the presence of crosswalks and pedestrian traffic.



PEDESTRIAN HYBRID BEACONS

Pedestrian Hybrid Beacons are useful in locations where drivers often fail to yield to pedestrians in traditional crosswalks, and where the deployment or cost of a full traffic signal would not be feasible. This includes mid-block crossings or uncontrolled mainline crossing points.

Prioritization Method

Projects were prioritized using a variety of factors matching the goals of the Active Living Initiative. The categories for prioritization are as follows:

- **Safety** – The project is located on a roadway with an observed high crash frequency, or is located on a roadway with higher levels of traffic stress, and should help to address these issues.
- **Access** – The project improves accessibility to key destinations via the bicycle or pedestrian network and connects to networks in incorporated cities or regional trails.
- **Equity** – The project is located within an area with a high need for active transportation. A composite equity score was calculated for each census tract using factors such as the proportion of the population with less than a high school education, who are linguistically isolated, under age 5, over age 64, considered low income, and who are people of color. Areas with composite scores that are higher than the scores of 60 percent of the rest of Harrison County are identified as “Areas of High Equity Need”.
- **Public Support** – While many factors affecting implementation cannot be quantified easily before prioritization, community support represents a critical element of project feasibility. Projects that are community-identified challenge areas or recommendations are also prioritized.

Each category was scored, and then weighted according to weights determined with the Active Living Committee.

CATEGORY	WEIGHT
Safety	30%
Access	20%
Equity	30%
Public Support	20%

Phased Project Recommendations

Once each project in the County was scored, projects with scores in the highest quartile which enhanced overall network connectivity in a logical fashion were assigned as Phase I projects. The project team encourages Harrison County and the local municipalities to focus on Phase I projects first. Phase II and Phase III projects had lower scores and represent an ideal build out scenario.





UNINCORPORATED HARRISON COUNTY

Projects reaching out into unincorporated Harrison County connect residents with key business and commercial areas near the coast. However, through the prioritization process, certain projects are recommended for implementation in the near term. Phase I projects for unincorporated Harrison County, otherwise known as near-term or highest priority projects, are shown in Figure 20. The highest priority projects are also shown in the darkest colors, with lower priority project shown in more faded hues.

The first projects that should be implemented in in unincorporated Harrison County include trails along Wolf River Road, Big Creek Road, County Farm Road, parallel to US-49, between

SR-67 and Lorraine Road, and as along Lamey Bridge Road. Bicycle facilities are recommended along Blackwell Farm Road, Success Road and Menge Avenue. Fewer sidewalk projects are recommended out in unincorporated Harrison County in Phase I.

Phase II projects primarily expand trails in unincorporated Harrison County out to the west, along SR-53, Kiln Delisle Road, Landon Road, Old US-49 and W. Wortham Road (Figure 21). Phase III projects continue the trend of adding new trail projects further inland. Trails are recommended along Vidalia Road, Edwin Ladner Road, Bic Creek Road, Saucier Lizana Road, Bethel Road, SR-605, SR-67, and SR-15. See Figure 22 for all the Phase III projects for unincorporated Harrison County.

Figure 20 Unincorporated Harrison County Phase I Recommendations

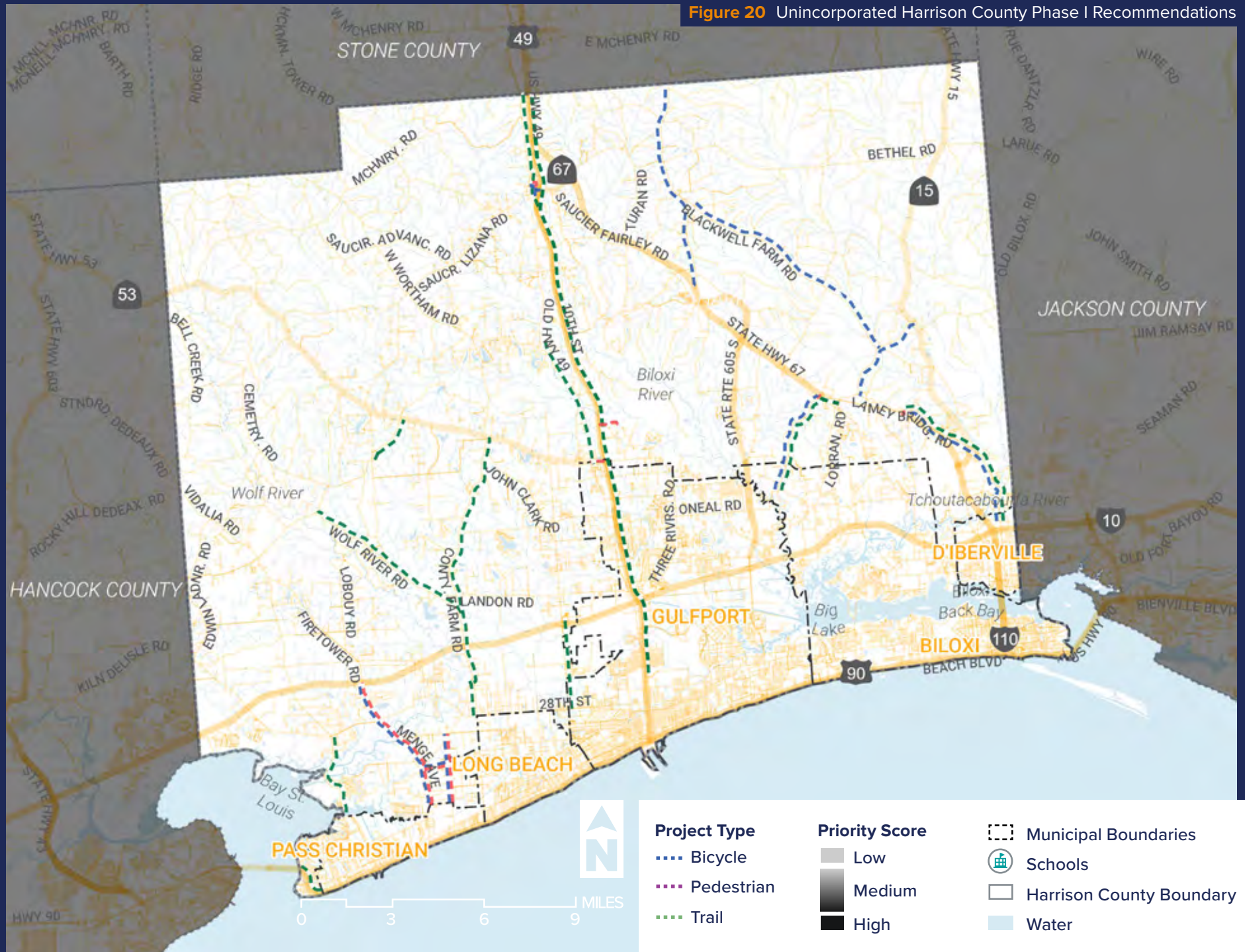


Figure 21 Unincorporated Harrison County Phase II Recommendations

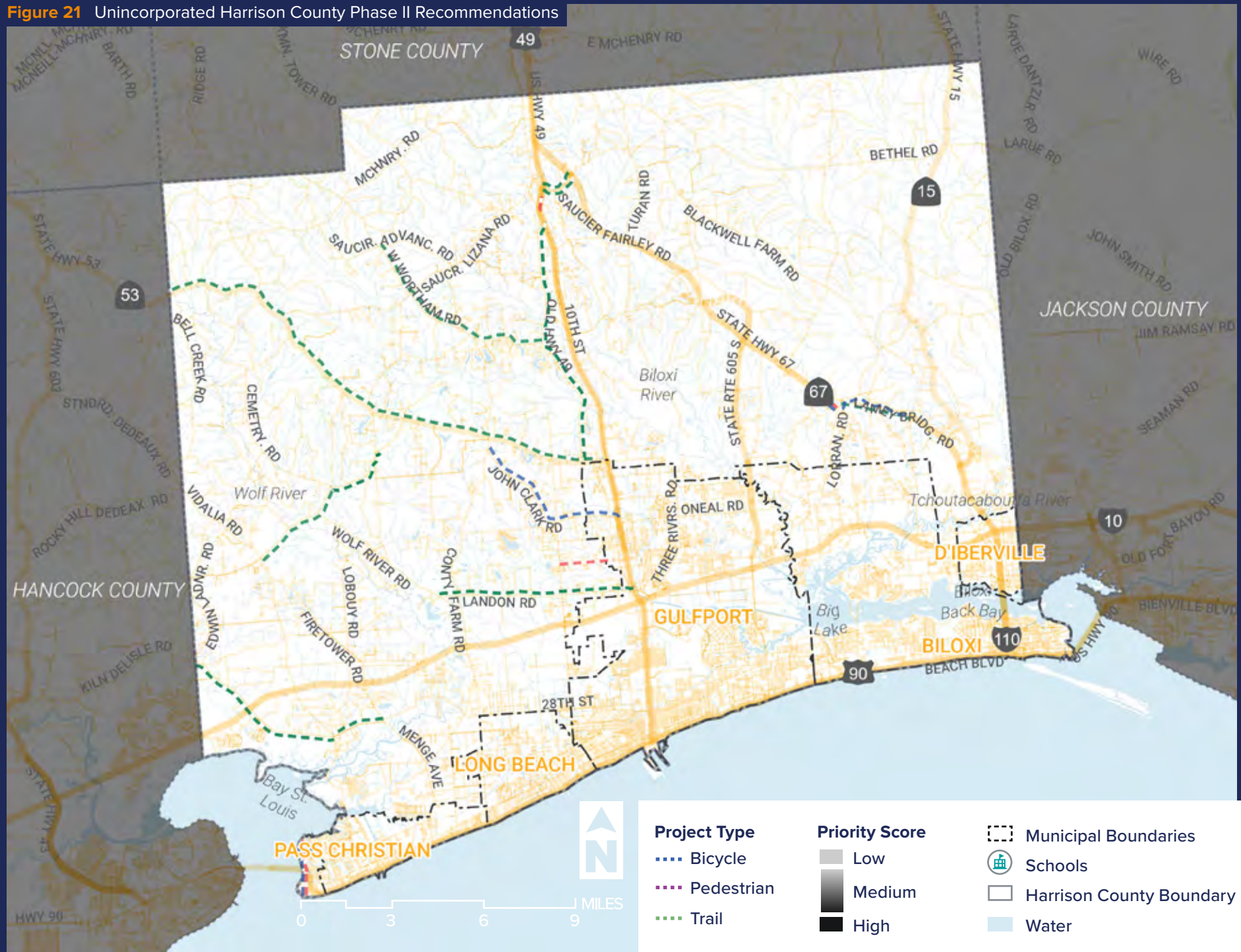
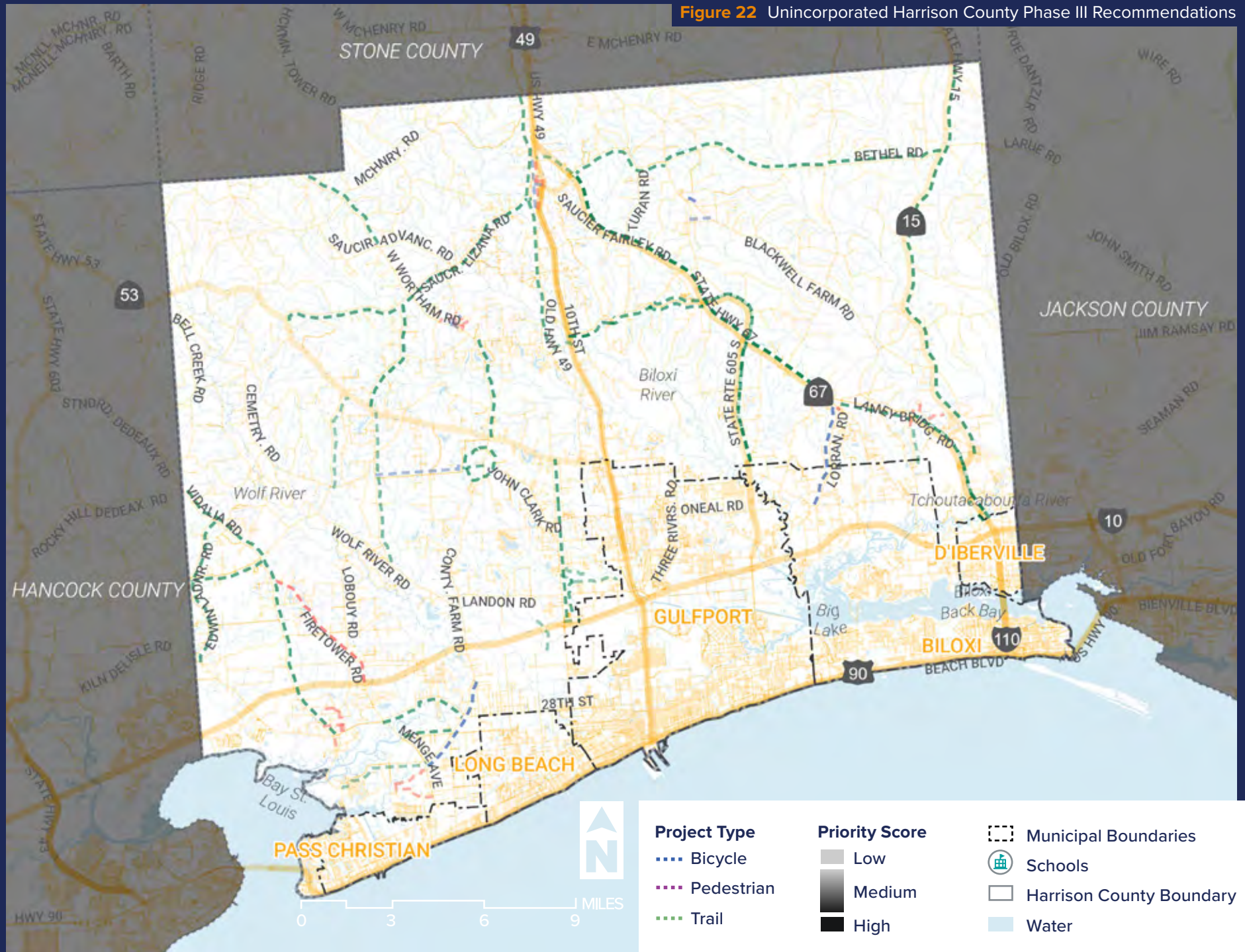


Figure 22 Unincorporated Harrison County Phase III Recommendations



UNINCORPORATED HARRISON COUNTY RECOMMENDATIONS

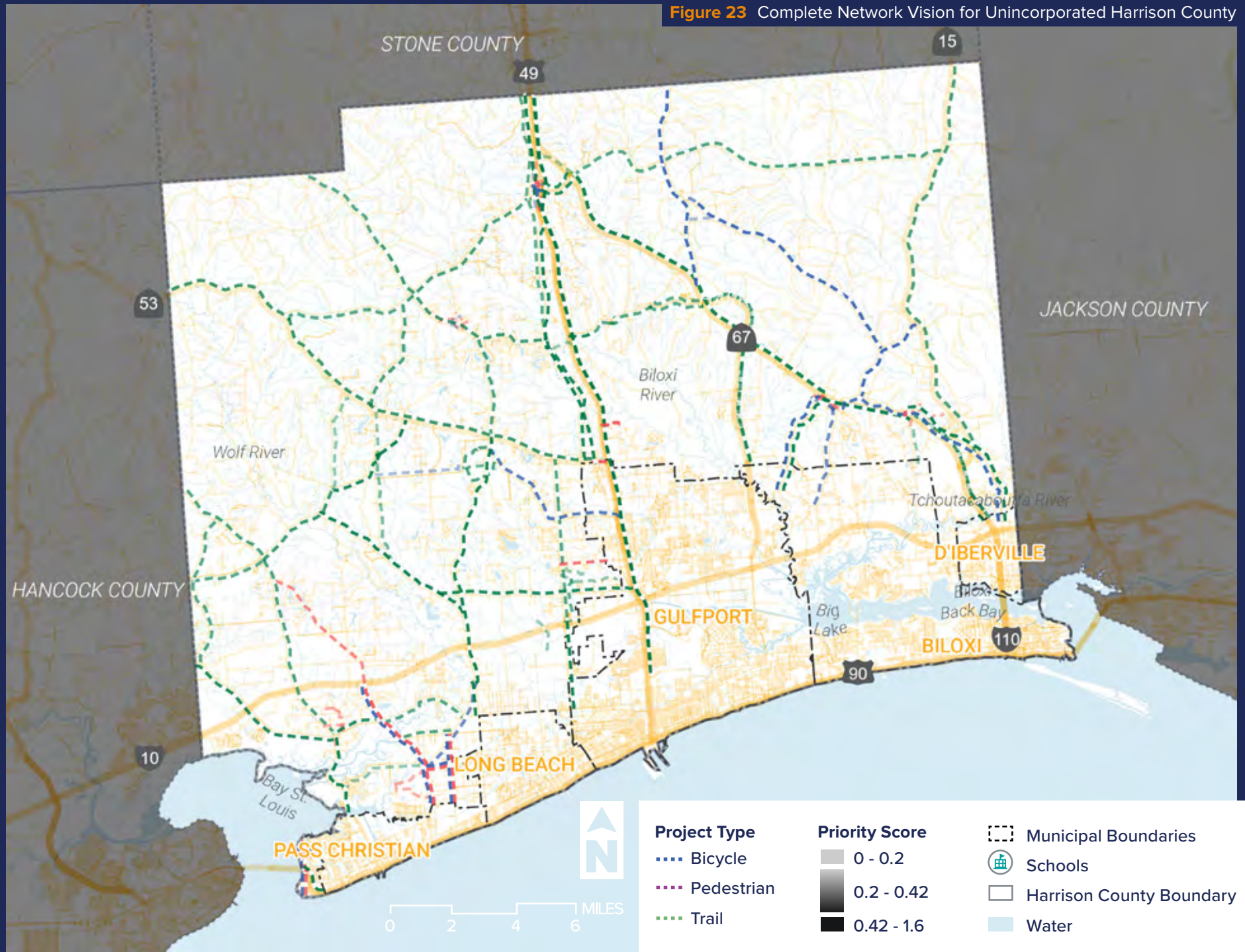
Phase I have prioritization score 0.42 and above

Phase II have prioritization score 0.34 to 0.41

Phase III have prioritization score 0.33 or less

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
W Beach Blvd	Lady Mary Ave	Sandy Hook Dr	Trail	1.18	I
10th St	Arkansas Ave	Northern County Border	Trail	19.60	I
School Rd	Highway 49	School Rd	Pedestrian	0.78	I
Kansas City Southern Railroad	Wortham	Lyman	Trail	4.40	I
10th St	Martin Luther King Jr. Blvd	Northern County Border	Trail	19.20	I
Cable Bridge Rd	Lizana School Rd	Big Creek Rd	Trail	0.28	I
State Hwy 53	Highway 49	Old Highway 49	Pedestrian	0.47	I
W Wittman Rd	Wolf River Bridge	Cuevas-Delisle Rd	Trail	1.70	I
Pineville Rd	Menge Ave	Espy Ave	Pedestrian	0.86	I
Pineville Rd	Menge Ave	Espy Ave	Bicycle	0.86	I
County Farm Rd	Red Creek Rd	Highway 53	Trail	8.42	I
Beatline Rd	Red Creek Rd	28th St	Trail	1.21	I
Service Rd North Of Old Woolmarket Rd	Switzer Park Rd	Old Woolmarket Rd	Pedestrian	0.16	I
Lamey Bridge Rd	Mallett Rd	Lanfair Rd	Bicycle	5.45	I
Lamey Bridge Rd	Mallett Rd	Lanfair Rd	Trail	5.45	I
Old Woolmarket Rd	John Lee Rd	Willow Oak Dr	Trail	3.65	I
Old Woolmarket Rd	John Lee Rd	Willow Oak Dr	Bicycle	3.65	I
W Wittman Rd	Wolf River Bridge	Wolf River Rd	Trail	1.19	I
Canal Rd	28th St	16th St	Trail	3.01	I
Cable Bridge Rd	Lizana School Rd	Highway 53	Trail	1.39	I
W Old Pass Rd	Klondyke Rd	N. Lang Ave	Pedestrian	1.06	I
1st St	Angel Rd	Old Highway 49	Pedestrian	0.37	I
1st St	Angel Rd	Old Highway 49	Bicycle	0.37	I

Figure 23 Complete Network Vision for Unincorporated Harrison County



UNINCORPORATED HARRISON COUNTY RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Airey Tower Rd	Bethel Rd	Northern County Border	Bicycle	2.62	I
Angel Rd	Highway 49	Church Ave	Bicycle	0.17	I
Beach Blvd	Boisdore Ave	Lady Mary Ave	Trail	0.05	I
Central Dr	Highway 49	Angel Rd	Bicycle	0.26	I
Espy Ave	Demourelle Rd	Freddie Frank Rd	Pedestrian	2.26	I
Espy Ave	Demourelle Rd	Freddie Frank Rd	Bicycle	2.26	I
Hwy 67	Old Woolmarket Rd	Shriners Blvd	Trail	0.62	I
Hwy 67	Rifle Range Rd	Lamey Bridge Rd	Trail	0.62	I
Lanfair Rd	Lamey Bridge Rd	Lanfair Rd South Of Highway 67	Pedestrian	0.21	I
Success Rd	Highway 67	Bethel Rd	Bicycle	4.48	I
Blackwell Farm Rd	Success Rd	White Plains Rd	Bicycle	7.94	I
Menge Ave	Demourelle Rd	Interstate 10	Pedestrian	4.85	I
Menge Ave	Demourelle Rd	Interstate 10	Bicycle	4.85	I
White Plains Rd	Highway 15	Lamey Bridge Rd	Bicycle	3.30	I
Old Hwy 49	Highway 49	Northern County Border	Trail	3.91	I
Wolf River Rd	County Farm Rd	Cable Bridge Rd	Trail	5.97	I
Old Hwy 49	Highway 53	Highway 49	Trail	8.53	II
Cable Bridge Rd	Cemetery Rd	Big Creek Rd	Trail	1.89	II
Cable Bridge Rd	Lizana School Rd	Cable Bridge Rd	Trail	0.13	II
Highway 49 Frontage Rd	Old Highway 49	Graham Dr	Pedestrian	0.49	II
Ponce De Leon Blvd	W. Beach Blvd	Sandy Hook Dr	Pedestrian	1.10	II
S Frontage Rd	Highway 49	Graham Dr	Pedestrian	0.47	II
Shortcut Rd	Lizana School Rd	Cable Bridge Rd	Trail	0.10	II
Willow Oak Dr	Lorraine Rd	Old Woolmarket Rd	Pedestrian	0.58	II
Willow Oak Dr	Lorraine Rd	Old Woolmarket Rd	Bicycle	0.58	II
Landon Rd	Highway 49	Wolf River Rd	Trail	6.40	II
Kiln Delisle Rd	Edwin Ladner Rd	Cuevas-Delisle Rd, W. Wittmann Rd, Vidalia Rd	Trail	5.45	II
Lamey Bridge Rd	Lanfair Rd, Lamey Bridge Rd	Willow Oak Dr, Lorraine Rd	Bicycle	2.66	II

UNINCORPORATED HARRISON COUNTY RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Pineville Delisle Rd	Menge Ave	Kiln-Delisle Rd, W. Wittmann Rd, Vidalia Rd	Trail	1.77	II
3rd Ave	Village Ln	Sandy Hook Dr, Ponce De Leon Blvd	Bicycle	1.87	II
Bethel Rd	Highway 67	Highway 49	Trail	1.24	II
State Hwy 67	Highway 49	Highway 67	Trail	1.22	II
John Clark Rd	County Farm Rd, Nicky Creek Rd, John Clark Rd	Old Highway 49, Clark Rd	Bicycle	5.51	II
3rd Ave	Village Ln	Ponce De Leon Blvd, W. Beach Blvd	Pedestrian	0.76	II
Orange Grove Rd	Canal Rd	Daisy Ln	Pedestrian	1.78	II
State Hwy 53	Old Highway 49	Harrison County line	Trail	16.19	II
Cable Bridge Rd	Vidalia Rd, J. P. Ladner Rd	Cemetery Rd	Trail	3.43	II
W Wortham Rd	Old Highway 49	Saucier Advance Rd	Trail	6.91	II
Hwy 67	Lamey Bridge Rd	Big John Rd	Trail	2.39	II
Red Creek Rd	Beatline Rd, County Farm Rd	Menge Ave	Bicycle	3.22	III
Lorraine Rd	Woolmarket Rd	Willow Oak Dr	Bicycle	3.38	III
State Hwy 67	Highway 15	Big John Rd	Trail	2.29	III
State Hwy 67	Old Woolmarket Rd, No Name	Bethel Rd	Trail	11.45	III
State Hwy 67	Rifle Range Rd, No Name	Bethel Rd	Trail	11.44	III
State Hwy 15	Melvin Rd, Saddle St	Harrison County line	Trail	13.95	III
Saucier Lizana Rd	Highway 53	Old Highway 49	Trail	9.58	III
Hwy 67	Shriners Blvd	Big John Rd	Trail	2.39	III
Bethel Rd	Highway 15	Airey Tower Rd, Success Rd	Trail	9.42	III
Vidalia Rd	Cuevas-Delisle Rd, Kiln-Delisle Rd, W. Wittmann Rd	Harrison County line	Trail	10.06	III
Shaw Rd	County Farm Rd, Highway 53	W. Wortham Rd, North Shaw Rd	Trail	3.41	III
State Hwy 67	Highway 15, Licksillet Rd	Big John Rd, Highway 67	Trail	3.44	III
Nicky Creel Trail	Mennonite Colony Rd	County Farm Rd	Trail	2.78	III
Bells Ferry Rd	Menge Ave	28th St, Red Creek Rd	Trail	3.23	III
Blackwell Farm Rd	Success Rd	Spikes Rd	Bicycle	0.33	III
Canal Rd	16th St	Inman Knoll Rd	Trail	3.54	III
Canal Rd	Inman Knoll Dr	Mark West Rd, Highway 53	Trail	1.53	III

UNINCORPORATED HARRISON COUNTY RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
E Wortham Dr	Saucier Rd	Saucier Rd, Road 416	Trail	0.19	III
E Wortham Rd	Highway 49, No Name	Saucier Rd	Trail	1.26	III
E Wortham Rd	Saucier Rd	Traditions Pkwy	Trail	1.45	III
E Wortham Rd	Saucier Rd, Road 416	Saucier Rd	Trail	0.20	III
Edwin Ladner Rd	Kiln-Delisle Rd	Vidalia Rd	Trail	5.19	III
Firetower Rd	Interstate 10, Menge Ave	Vidalia Rd	Pedestrian	4.84	III
Freddie Frank Rd	Red Creek Rd, Daugherty Rd	Menge Ave	Trail	1.56	III
Havana Blvd	Ponce De Leon Blvd, Havana Blvd	Fairway Dr, Island Estates Dr	Bicycle	0.28	III
Herman Ladner Rd	Highway 53	Saucier Lizana Rd	Trail	4.11	III
Joe Pete Ladner Rd	Edwin Ladner Rd	Vidalia Rd, Cable Bridge Rd	Trail	2.42	III
Old Hwy 67	0.14 W of SR-15	State Hwy 15	Trail	0.14	III
Old Hwy 67	Highway 67	Airey Tower Rd, Success Rd	Trail	3.76	III
Palmer Creek	Highway 49, No Name	E. Wortham Rd, Scarborough Rd	Trail	0.78	III
S R 605 N	Highway 605	Highway 605, Doby Dr	Trail	1.18	III
State Hwy 15	Brandon James Dr	Rue Sanchez	Trail	0.33	III
State Hwy 15/67	Brandon James Dr	Lickskillet Rd	Trail	0.59	III
State Hwy 15/67	Rue Sanchez	Highway 67	Trail	1.72	III
State Hwy 605	Doby Dr	White Star Rd	Trail	1.25	III
State Hwy 605	Highway 605	Highway 67, Traditions Pkwy	Trail	2.98	III
State Rte 605 S	Highway 605	Traditions Pkwy, Highway 67	Trail	6.76	III
Switzer Park Rd	Old Woolmarket Rd	No Name	Pedestrian	0.44	III
Switzer Park Rd	Old Woolmarket Rd	No Name	Bicycle	0.44	III
Traditions Pkwy	E. Wortham Rd	Highway 67	Trail	1.81	III
W Wortham Rd	Saucier-Advance Rd	No Name	Trail	3.65	III
Willow Oak Dr	Shriners Blvd	Lorraine Rd	Pedestrian	0.13	III
Kansas City Southern Railroad	0.2 N of W Wortham Rd	Harrison County line	Trail	8.19	III
Greenway Trail 1	Greenway 2	16th St, Melissa St, J. F. M. Pkwy	Trail	1.32	III
County Fairgrounds Trail	Mennonite Rd	County Farm Rd	Trail	0.71	III
Big Creek Rd	Wolf River Rd	Cable Bridge Rd	Trail	3.46	III
MGCCC West Harrison Internal Road	Pineville Rd	B St	Bicycle	0.22	III
2nd St	Highway 49, No Name	Old Highway 49	Pedestrian	0.35	III

UNINCORPORATED HARRISON COUNTY RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
2nd St	Highway 49, No Name	Old Highway 49	Bicycle	0.35	III
B St	Espy Ave	J. E. L. Dr	Bicycle	0.05	III
B St	J. E. L. Dr	A Ave	Bicycle	0.31	III
B St	J. E. L. Dr	Espy Ave	Bicycle	0.05	III
Behind Lizana Elementary School	Cable Bridge Rd	Cable Bridge Rd	Trail	0.96	III
Church Ave	Old Highway 49	No Name, Highway 49	Bicycle	1.62	III
Church Ave	Old Highway 49	No Name, Highway 49	Pedestrian	1.62	III
Hand Rd	Lechene Dr	Cuevas-Delisle Rd	Pedestrian	0.35	III
Lennis Cuevas Rd	W. Wortham Rd	Nells Rd	Pedestrian	0.47	III
Lennis Cuevas Rd	W. Wortham Rd	Nells Rd	Bicycle	0.47	III
Mennonite Rd	Big Creek Rd	County Fairgrounds Trail	Bicycle	3.10	III
Montebella Rd	Menge Ave	Woodland Way	Pedestrian	1.59	III
Ollie Ln	Lamey Bridge Rd	Overland Dr	Pedestrian	0.41	III
Rd 514	Frontage Rd, Bethel Rd	Road 514, Highway 49	Pedestrian	0.45	III
Rudd Dr	Wallace Way	Benson Pl	Pedestrian	0.36	III
School Rd	Success Rd	Blackwell Farm Rd	Bicycle	0.82	III
W Wortham Rd	No Name, Highway 49	Old Highway 49, Robinwood Dr	Trail	0.80	III
Wallace Way	Wallace Way, W. Wortham Rd	Jarrett Dr, Waltrip Way, Stewart St, Skinner Ct	Pedestrian	0.53	III
Wallace Way	Wallace Way, W. Wortham Rd	Jarrett Dr, Waltrip Way, Stewart St, Skinner Ct	Bicycle	0.53	III
Woodland Way	Montebella Rd	Menge Ave, Pineville Rd	Pedestrian	1.21	III
Bernad, Bayou Greenway 1	Canal Rd, Cypress Pointe Dr	Greenway 1	Trail	0.58	III
Turkey Creek Greenway	East of N Klondyke Rd	0.25 W Big Leaf Dr	Trail	1.36	III
Peyton Greenway	Peyton Dr, Orange Grove Rd	Bernard, Bayou Greenway 2	Trail	0.93	III
Bernard, Bayou Greenway 2	Greenway 1	Coleman Rd, North April Dr	Trail	1.73	III
Lizana School Rd	Cable Bridge Rd, Big Creek Rd	Lizana School Rd, Highway 53	Trail	1.53	III
Lobouy Rd	Magnolia Blvd	Cuevas-Delisle Rd	Pedestrian	0.75	III
Saint Stephens Rd	Vidalia Rd	Frederick Rd	Pedestrian	0.80	III
Lamey Bridge Connector	Lamey Bridge Rd	Avondale Cir	Pedestrian	0.58	III
Big John Rd	Herbie Rd	Lamey Bridge Rd	Pedestrian	0.54	III

UNINCORPORATED HARRISON COUNTY RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Gordon St	Lennis Cuevas Rd	W. Wortham Rd	Pedestrian	0.59	III
Oak Rd	Cuevas-Delisle Rd	Mahoney Dr	Pedestrian	0.50	III
Arcadia Farm Rd	W. Wittmann Rd, Wolf River Ridge Rd	Menge Ave	Trail	3.26	III
16th St	Canal Rd	J. F. M. Pkwy, Melissa St	Trail	0.56	III
Big John Rd	Schonewitz Rd	Herbie Rd	Trail	1.23	III
Biloxi Cutoff Rd	Bethel Rd, Biloxi-Cutoff Rd	Church Ave, Highway 49	Trail	1.27	III
Magnolia Blvd	Lobouy Rd	Magnolia Lp	Pedestrian	0.24	III
Magnolia Loop	Magnolia Blvd	Dogwood Pl, Dogwood Cir, Walnut Rd, Gardenia Pl, Cedar Ln	Pedestrian	0.40	III
Northrup Cuevas Rd	Cemetery Rd	Highway 53	Trail	3.29	III





CITY OF BILOXI

The Initiative recommends that Biloxi prioritize projects near the coast, then move northward as implementation progresses. The projects are divided into different phases, with Phase I envisioned as near-term or highest priority projects. The highest priority projects are shown in the darkest colors, with lower priority shown in more faded hues. Phase I projects are shown in Figure 24.

Key projects for Biloxi include a trail, either a sidepath or greenway, along both Pass Road and Bayview Avenue; north-south bicycle facilities along Debuys Road, Beauvoir Road, Rodenberg Avenue, Caillavet Street, Anglada Street, Reynoir Street, Main Street, Nichols Drive, and Oak Street; as well as east-west bicycle facilities along Division Street and Howard Avenue. Many of these Phase I projects can be better seen in Figure 25. There are six different spot improvements recommended in Biloxi along US-90 /

Beach Boulevard: mid-block crossings near Caldwell Avenue, Seashore Oaks Assisted Living, Veterans Avenue, and Briarfield Avenue; and intersection improvements for pedestrians at Rodenberg Avenue and SR-15.

Phase II projects in Biloxi include a trail along Popp's Ferry Road north of the Back Bay, as well as a trail along Irish Hill Drive. Additional bicycle facilities and sidewalks are also recommended in this phase, as shown in Figures 26-27. Phase III projects expand on the Phase II projects with an additional trail recommended along Irish Hill Drive, as well as more recommendations north of the Back Bay such as bicycle facilities along Brodie Road. Trails are also recommended north of the Bay in Phase III along the Popp's Ferry Bridge, Cedar Lake Road, and Woodmarket Road. Phase III projects are shown in Figure 28.

Figure 24 Biloxi Phase I Recommendations

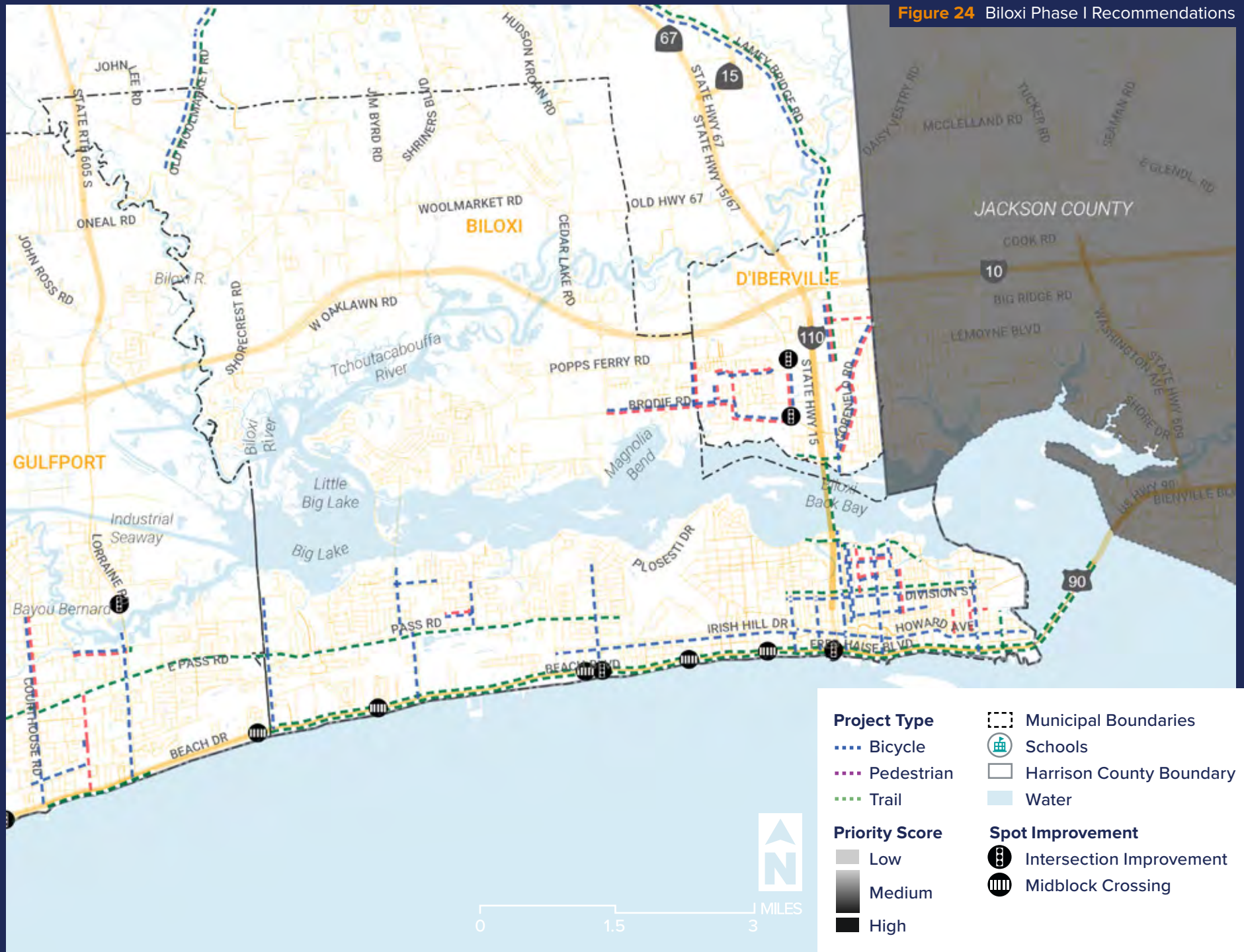


Figure 25 Biloxi Phase I Recommendations Detail

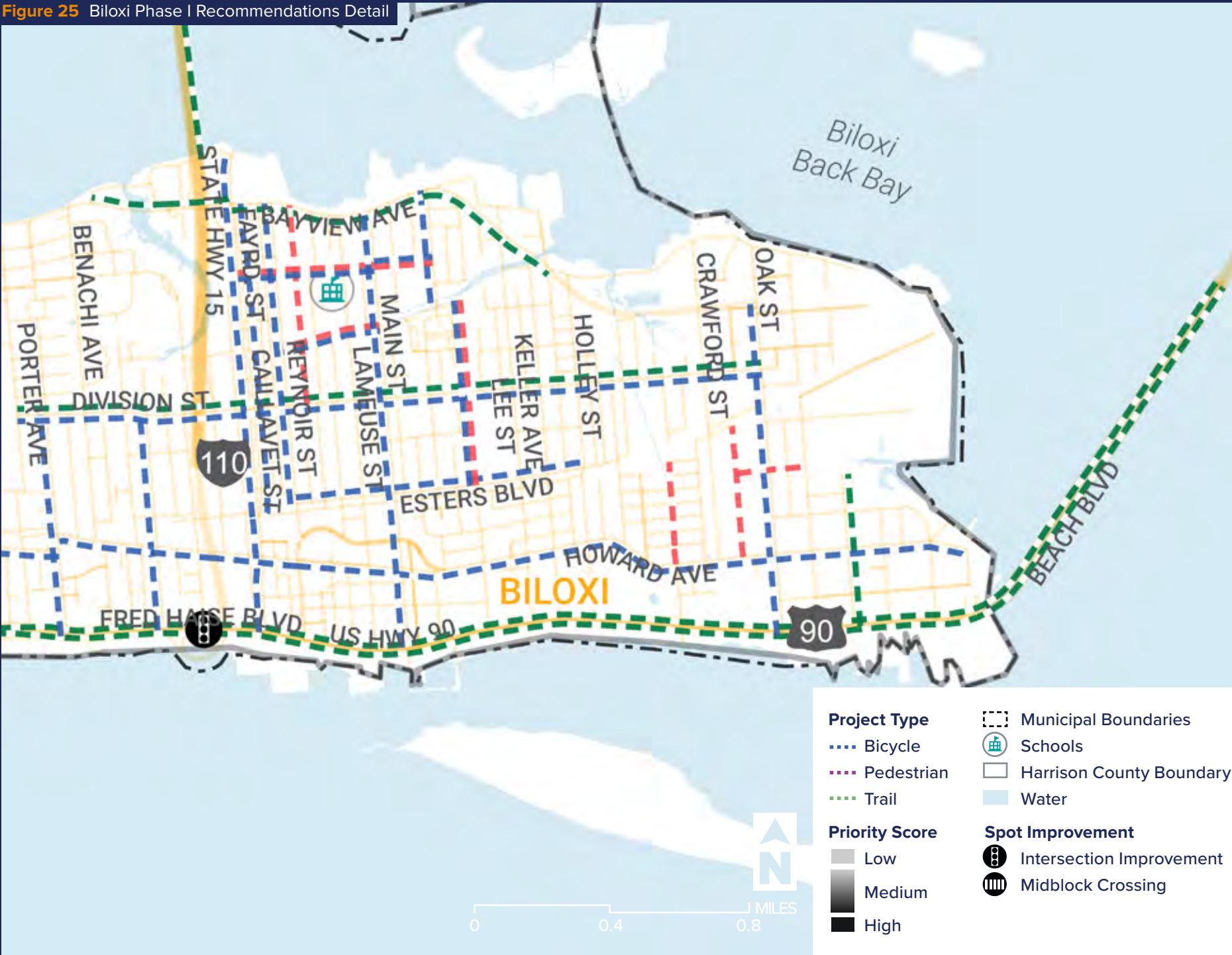


Figure 26 Biloxi Phase II Recommendations

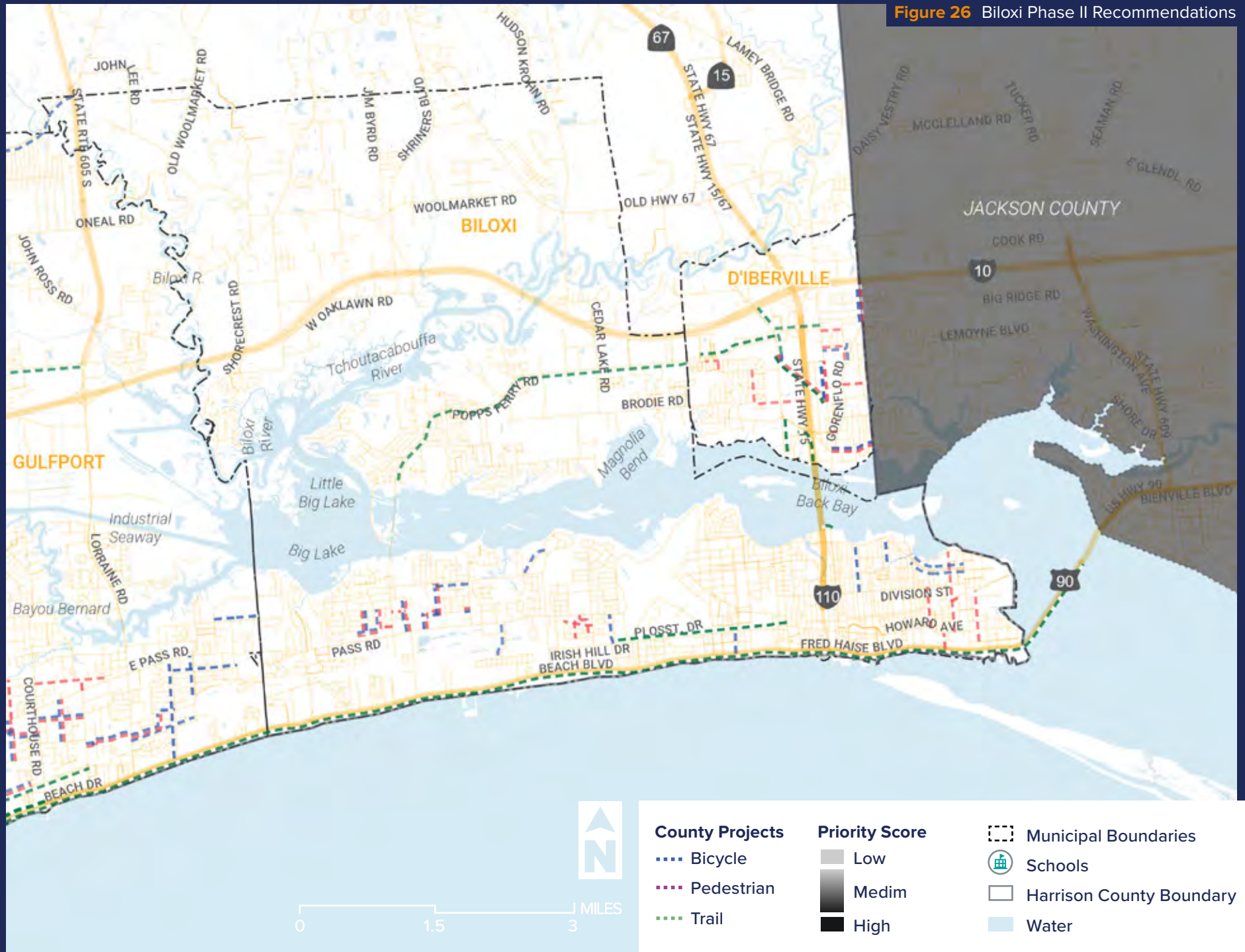
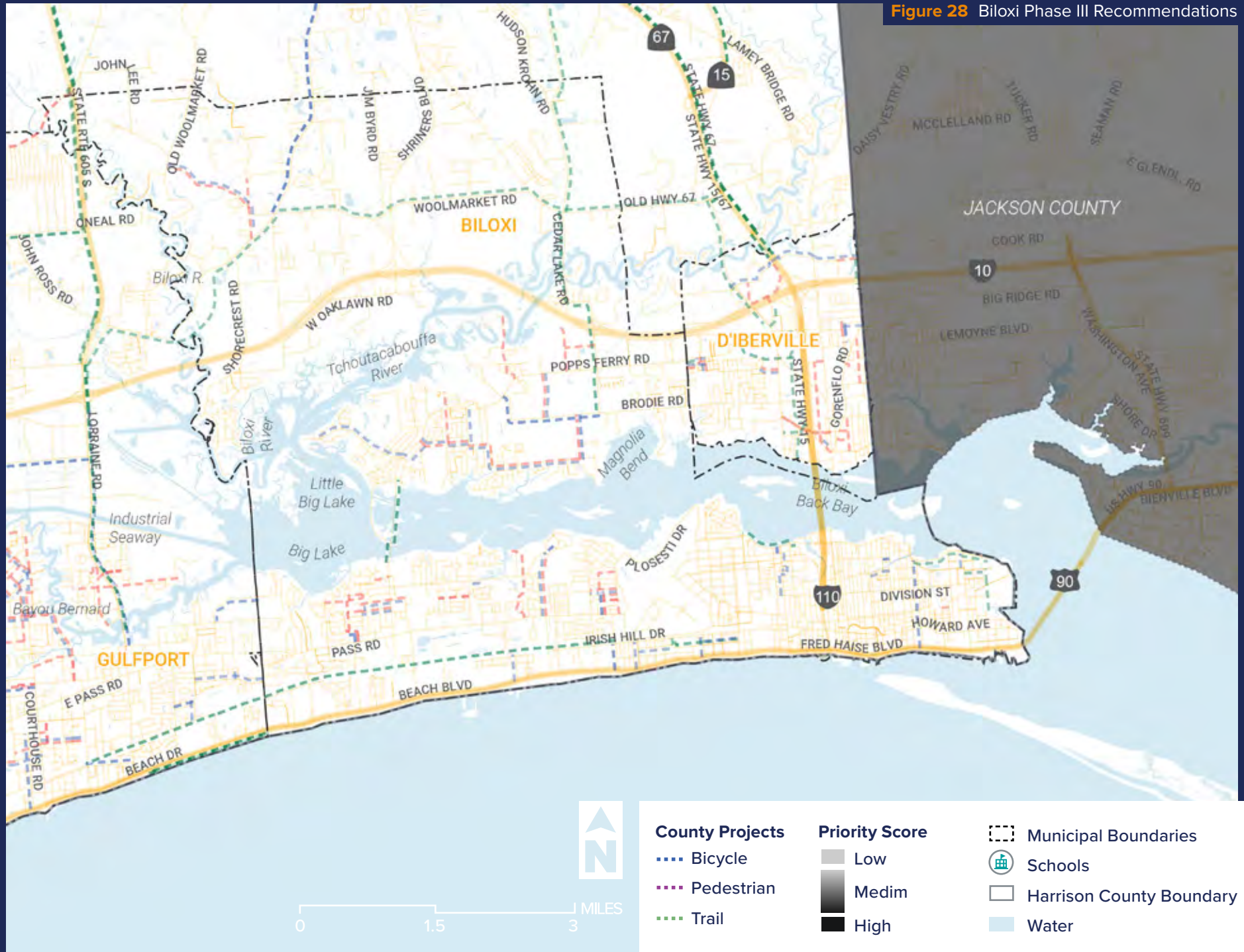


Figure 27 Biloxi Phase II Recommendations Detail



Figure 28 Biloxi Phase III Recommendations



CITY OF BILOXI RECOMMENDATIONS

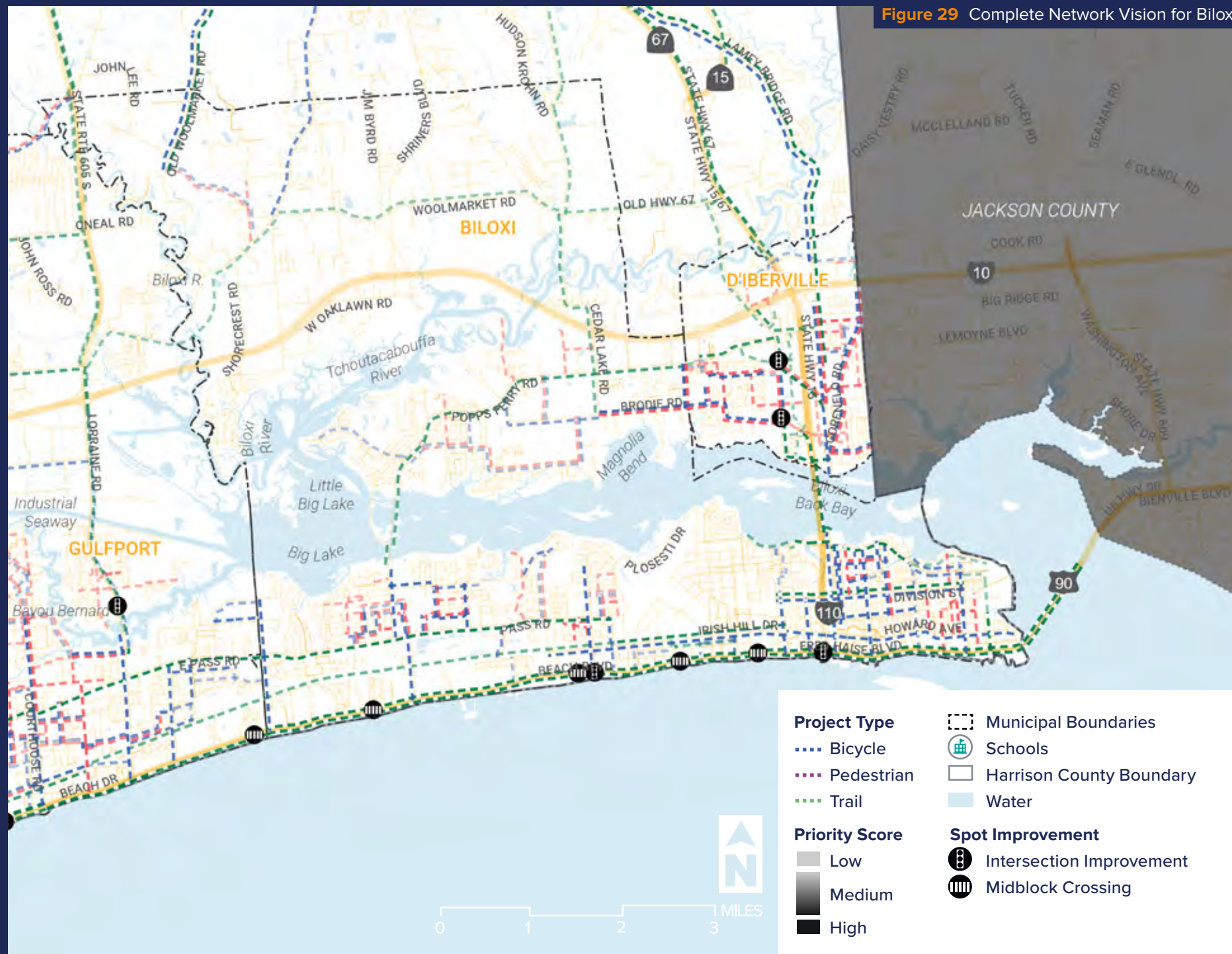
Phase I have prioritization score 0.42 and above

Phase II have prioritization score 0.34 to 0.41

Phase III have prioritization score 0.33 or less

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
State Hwy 15, I-110 Bridge Over Back Bay	Bayshore Dr	Bay Shore Dr	Trail	0.79	I
Main St	Beach Blvd	Bayview Ave	Bicycle	1.25	I
Bayview Ave	Caillavet St	Braun St	Trail	0.57	I
Bayview Ave	Caillavet St	Diaz Ave	Trail	0.18	I
Caillavet St	Beach Blvd	Caillavet St	Bicycle	0.34	I
Esters Blvd	Reynoir St	Holley St	Bicycle	0.83	I
Popps Ferry Rd	Anatole Bay Townhomes	Pass Rd	Bicycle	0.77	I
Howard Ave	Cadet St	Reynoir St	Bicycle	1.94	I
Pine St	Beach Blvd	Tchula St	Trail	0.43	I
Pass Rd	Debuys Rd	Iberville Dr	Trail	4.01	I
E 4th St	Hoxie St	Maple St	Pedestrian	0.20	I
Walker St	Main St	Braun St	Pedestrian	0.17	I
Walker St	Main St	Braun St	Bicycle	0.17	I
Caillavet St	Bayview Ave	Esters Blvd	Bicycle	0.89	I
Caillavet St	Esters Blvd	Bayshore Dr	Bicycle	1.02	I
Jim Money Rd	Pass Rd	Atkinson Rd	Bicycle	0.50	I
Division St	Oak St	Forrest Ave	Trail	2.13	I
Division St	Oak St	Forrest Ave	Bicycle	2.13	I
Back Bay Blvd	Bayview Ave	Hill St	Trail	0.28	I
Bayview Ave	Diaz Ave	Bayshore Dr	Trail	0.22	I
Kuhn St	Howard Ave	Clay St	Pedestrian	0.05	I
Nichols Dr	Railroad St	Bradford St	Pedestrian	0.53	I
Nichols Dr	Railroad St	Bradford St	Bicycle	0.53	I
Oak St	Beach Blvd	Back Bay Blvd	Bicycle	0.94	I
Debuys Rd	Pass Rd	Beach Blvd	Bicycle	0.87	I
Bradford St	Main St	Reynoir St	Pedestrian	0.28	I

Figure 29 Complete Network Vision for Biloxi



CITY OF BILOXI RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Bradford St	Main St	Reynoir St	Bicycle	0.28	I
Braun St	Bradford St	Bayview Ave	Bicycle	0.33	I
Croesus St	Division St	Bayview Ave	Pedestrian	0.57	I
Debuys Rd	Runnymede Dr	Pass Rd	Bicycle	0.62	I
Elder St	Main St	Reynoir St	Pedestrian	0.28	I
Elder St	Main St	Reynoir St	Bicycle	0.28	I
Elder St	Reynoir St	Caillavet St	Pedestrian	0.11	I
Elder St	Reynoir St	Caillavet St	Bicycle	0.11	I
Floyd Dr	Driftwood Dr	Greenwood Dr	Pedestrian	0.30	I
Floyd Dr	Driftwood Dr	Greenwood Dr	Bicycle	0.30	I
Reynoir St	Railroad St	Bayview Ave	Bicycle	0.86	I
Irish Hill Dr	Porter Ave	Veterans Ave	Bicycle	2.71	I
Beach Blvd	Debuys Rd	Jackson County	Trail	9.76	I
Beauvoir Rd	Beach Blvd	Pass Rd	Bicycle	0.75	I
Rodenberg Ave	Beach Blvd	Maltby Hall Dr	Bicycle	1.25	I
W Howard Ave	Interstate 110	Porter Ave	Bicycle	0.45	I
Porter Ave	Beach Blvd	Division St	Bicycle	0.63	I
Beach Blvd	Debuys Rd	Jackson County	Trail	9.77	I
Atkinson Rd	Popps Ferry Rd	Acadian Ct, Linda Dr	Bicycle	0.49	I
Back Bay Blvd	Hill St	Bayview Ave	Trail	0.16	I
Hoxie St	Howard Ave	Railroad St	Pedestrian	0.37	I
Kuhn St	Clay St	Bonner Dr	Pedestrian	0.24	I
Seal Ave	Beach Blvd	Division St	Bicycle	0.62	I
Porter Ave	Irish Hill Dr	Iberville Dr	Trail	2.03	II
Agincourt Ave	Pass Rd	Stelly Dr	Pedestrian	0.16	II
Agincourt Ave	Pass Rd	Stelly Dr	Bicycle	0.16	II
Bay Shore Dr	Caillavet St	I-110	Trail	0.10	II
Carter Rd	Bay Vista Dr	Randall Dr	Pedestrian	0.27	II
Carter Rd	Bay Vista Dr	Randall Dr	Bicycle	0.27	II
Dewey Cir	Ridgeway Dr	Garden Park Dr	Pedestrian	0.48	II
Dorries St	Howard Ave	Division St	Pedestrian	0.47	II
Dr Gilbert Mason Dr	Splendor St	Back Bay Blvd	Bicycle	0.24	II
Driftwood Dr	Jim Money Rd	Driftwood Dr	Pedestrian	0.06	II

CITY OF BILOXI RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Driftwood Dr	Jim Money Rd	Driftwood Dr	Bicycle	0.06	II
Greenwood Dr	Floyd Dr	Atkinson Rd	Pedestrian	0.37	II
Greenwood Dr	Floyd Dr	Atkinson Rd	Bicycle	0.37	II
Hinman Dr	Popps Ferry Rd	Randall Dr	Pedestrian	0.13	II
Iberville Dr	Irish Hill Dr	Pass Rd	Bicycle	0.33	II
Julia St	Dewey Cir	Pass Rd	Pedestrian	0.06	II
Kuhn St	Beach Blvd	Howard Ave	Pedestrian	0.18	II
Lameuse St	Division St	Bayview Ave	Bicycle	0.52	II
Nelson Rd	Pass Rd	Carter Rd	Pedestrian	0.40	II
Nelson Rd	Pass Rd	Carter Rd	Bicycle	0.40	II
Railroad St	Hoxie St	Rosetti St	Pedestrian	0.11	II
State St	Rodenberg Ave	Bleuer Dr	Pedestrian	0.33	II
Stelly Dr	Greenwood Dr	Agincourt Ave	Pedestrian	0.16	II
Stelly Dr	Greenwood Dr	Agincourt Ave	Bicycle	0.16	II
Trafalgar Dr	Nelson Rd	Popps Ferry Rd	Pedestrian	0.16	II
White Ave	Beach Blvd	Irish Hill Dr	Bicycle	0.26	II
Maple St	1st St	Michael Industrial Blvd	Pedestrian	0.57	II
Fairchild Dr	Atkinson Rd	Veterans Ave	Bicycle	0.34	II
Atkinson Rd	Jim Money Rd	Acadian Ct, Linda Dr	Bicycle	0.40	II
Back Bay Blvd	Bayview Ave	North Of Penny Ave	Bicycle	0.29	II
Back Bay Blvd	North Of Penny Ave	North Of Bowen St	Bicycle	0.29	II
Back Bay Blvd	North Of Penny Ave	Oak St	Bicycle	0.33	II
Back Bay Blvd	Oak St	North Of Penny Ave	Bicycle	0.33	II
Crawford St	Howard Ave	Bayview Ave	Pedestrian	0.91	II
Randall Dr	Macarthur Ave	Bay Vista Dr	Pedestrian	0.51	II
Randall Dr	Macarthur Ave	Bay Vista Dr	Bicycle	0.51	II
Popps Ferry Rd	Causeway Dr	Lamey St, Gay Rd	Trail	3.94	II
Iris St	Greater Ave	Pass Rd	Bicycle	0.39	II
Alice Dr	Jim Money Rd	Greenwood Dr	Pedestrian	0.35	III
Iberville Dr	Beach Blvd	Irish Hill Dr	Bicycle	0.30	III
Macarthur Ave	Pass Rd	Randall Dr	Pedestrian	0.06	III
Macarthur Ave	Pass Rd	Randall Dr	Bicycle	0.06	III

CITY OF BILOXI RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Macarthur Ave	Randall Dr	Brighton Cir	Pedestrian	0.19	III
Macarthur Ave	Randall Dr	Brighton Cir	Bicycle	0.19	III
Popps Ferry Bridge	Causeway Dr	Causeway Dr	Trail	0.72	III
Saint Mary Blvd	Pass Rd	Wiltshire Blvd	Pedestrian	0.28	III
Saint Mary Blvd	Pass Rd	Wiltshire Blvd	Bicycle	0.28	III
Splendor St	Braun St	Dr. Gilbert Mason Dr	Bicycle	0.09	III
St Martha St	Rodeo Dr	St. Mary Blvd	Pedestrian	0.15	III
Tribe Dr	Cedar Lake Rd	Popps Ferry Rd	Bicycle	1.32	III
Veterans Ave	Fairchild Dr	Gulf Coast Veterans Healthcare System	Bicycle	0.14	III
Rail Corridor	Iberville Dr	Great Southern Golf Club Railroad Crossing	Trail	4.61	III
Cedar Lake Rd	Old Highway 67	Brodie Rd	Trail	2.48	III
Bay Shore Dr	Benachi Ave	Bayshore Dr	Trail	0.07	III
Bayview Ave	Benachi Ave	Kensington Dr	Trail	0.58	III
Churchill Ave	Macarthur Ave	Old Bay Rd	Pedestrian	0.34	III
Father Ryan Ave	St. Francis St	Miller St	Trail	0.34	III
Father Ryan Ave	St. Francis St	White Ave	Bicycle	0.39	III
Father Ryan Ave	White Ave	St. Francis St	Trail	0.39	III
Holley St	Beach Blvd	Bayview Ave	Bicycle	0.99	III
Saint Francis St	Beach Blvd	Irish Hill Dr	Bicycle	0.25	III
Wells Dr	Richard Dr	Brasher Rd	Bicycle	0.51	III
Wells Dr	Richard Dr	Kennedy Ln	Pedestrian	0.51	III
Hiller Dr	Pass Rd	Creel Rd	Pedestrian	0.92	III
Veterans Ave	Pass Rd	Fairchild Dr	Pedestrian	0.70	III
Veterans Ave	Pass Rd	Fairchild Dr	Bicycle	0.70	III
Bonner Dr	Kuhn St	Rosetti St	Pedestrian	0.08	III
Bradford St	Nichols Dr	Main St	Bicycle	0.24	III
Camellia St	Beach Blvd	Greater Ave	Bicycle	0.30	III
E 8th St	Oak St	Pine St	Trail	0.21	III
Hollywood Blvd	Rodeo Dr	Wiltshire Blvd	Pedestrian	0.37	III
Oak St	Back Bay Blvd	Bayview Ave	Bicycle	0.16	III
Old Bay Rd	Linwood Dr	Big Lake Rd	Pedestrian	0.49	III
Park Ct N	Park Ct N	Park Ct S	Bicycle	0.09	III

CITY OF BILOXI RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Park Ct N	Park Ct N	Park Ct S	Bicycle	0.05	III
Park Ct S	Park Ct S	Porter Ave	Bicycle	0.04	III
Pine St	Southern End Of Street	8th St	Trail	0.16	III
Pine Street Connector	Tchula St	7th St	Trail	0.32	III
Porter Ave	Division St	Park Ct N.	Bicycle	0.27	III
Porter Ave	Lafayette St	Park Ct N	Bicycle	0.15	III
Rodeo Dr	Pass Rd	St. Martha Ave	Pedestrian	0.17	III
Rustwood Dr	Trailwood Dr	Lauren Dr	Pedestrian	0.57	III
St Ann Ave	Rodeo Dr	St. Mary Blvd	Pedestrian	0.15	III
Wiltshire Blvd	Rodeo Dr	Rodenberg Ave	Pedestrian	0.30	III
Wiltshire Blvd	Rodeo Dr	Rodenberg Ave	Bicycle	0.30	III
Lorraine Rd	Helen Richards Rd	Woolmarket Rd	Trail	3.37	III
Hudson Krohn Rd	Old Highway 67	Schonewitz Rd	Trail	1.18	III
Carter Rd	Nelson Rd	Bay Vista Dr	Pedestrian	0.57	III
Carter Rd	Nelson Rd	Bay Vista Dr	Bicycle	0.57	III
John Lee Rd	Old Woolmarket Rd	Lorraine Rd	Pedestrian	1.44	III
John Lee Rd	Old Woolmarket Rd	Lorraine Rd	Bicycle	1.35	III
Woolmarket Rd	Old Highway 67	Lorraine Rd	Trail	2.95	III
Bay Vista Dr	Carter Rd	Churchill Ave	Pedestrian	0.16	III
Bay Vista Dr	Popps Ferry Rd	Carter Rd	Pedestrian	0.33	III
Bay Vista Dr	Popps Ferry Rd	Carter Rd	Bicycle	0.33	III
Linwood Dr	Linwood Dr	Linwood Dr	Pedestrian	0.20	III
Miller St	Father Ryan Ave	Summer Pl	Trail	0.08	III
Miller St	Summer Pl	St. Charles Ave	Bicycle	0.35	III
St Charles Ave	Beach Blvd	Irish Hill Dr	Bicycle	0.31	III
Summer Pl	Miller St	Irish Hill Dr	Trail	0.08	III
Cedar Lake Rd	Popps Ferry Rd, Ellzey Dr	Jam Ln, Richard Dr	Pedestrian	0.50	III
Cedar Lake Rd	Popps Ferry Rd, Ellzey Dr	Jam Ln, Richard Dr	Bicycle	0.50	III
Bayview Ave	Back Bay Blvd	8th St, Pine St	Trail	0.89	III
Brasher Rd	Wells Dr	Camp Wilkes Rd, Brasher Rd	Pedestrian	1.47	III
Brasher Rd	Wells Dr	Camp Wilkes Rd, Brasher Rd	Bicycle	1.47	III
Camp Wilkes Rd	Popps Ferry Rd	Brasher Rd	Bicycle	0.41	III
Old Hwy 67 Connector	Old Hwy 67	Woolmarket Rd	Trail	2.13	III

CITY OF BILOXI RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Porter Ave	Lafayette St	Bayview Ave	Bicycle	0.07	III
Porter Ave	Lafayette St	Bayview Ave	Bicycle	0.07	III
State Rte 605 N	0.2 Miles North of River Rd	Three Rivers Rd	Trail	0.68	III
Sunkist Country Club Rd	Sunkist Country Club Rd	Baywood Dr	Bicycle	0.10	III
Three Rivers Rd Connector	Three Rivers Rd	State Rte 605	Pedestrian	0.15	III
Atkinson Rd	Mcarnay Dr	Jim Money Rd	Bicycle	0.13	III
Bonne Terra Blvd	Carmargue Ln	Popps Ferry Rd	Pedestrian	0.16	III
Wash Fayard Rd	John Lee Rd	Beaver Trl	Pedestrian	0.33	III
Tommy Monroe Dr	Popps Ferry Rd	Interstate 10	Pedestrian	0.55	III
Campbell Dr	Popps Ferry Rd	End of Campbell Dr	Pedestrian	0.82	III
Baywood Dr	Rustwood Dr	No Name	Bicycle	0.26	III
Medical Park Dr	Elizabeth Blvd, Cedar Lake Rd	Medical Park Dr, Tommy Munro Dr	Pedestrian	0.34	III
Rustwood Dr	Baywood Dr	Trailwood Dr	Pedestrian	0.31	III
Rustwood Dr	Baywood Dr	Trailwood Dr	Bicycle	0.31	III
Rustwood Dr	Popps Ferry Rd	Baywood Dr	Pedestrian	0.05	III
Rustwood Dr	Popps Ferry Rd	Baywood Dr	Bicycle	0.05	III
Schonewitz Rd	Hudson Krohn Rd	Big John Rd	Trail	0.26	III
Baywood Dr	Sunkist Country Club Rd	Rustwood Dr	Bicycle	1.01	III
Trailwood Dr	Camp Wilkes Rd	Rustwood Dr	Pedestrian	0.23	III
Trailwood Dr	Camp Wilkes Rd	Rustwood Dr	Bicycle	0.23	III

SPOT IMPROVEMENTS

The following table outlines key spot improvements for the City of Biloxi.

LOCATION	CITY	TYPE	DESCRIPTION
US-90 / Beach Blvd and Rodenberg Ave	Biloxi	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
I-110 and US-90 / Beach Blvd	Biloxi	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
US-90 between Caldwell Ave & Azalea Dr	Biloxi	Mid-block Crossing	Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.
US-90 at Seashore Oaks Assisted Living Driveway	Biloxi	Mid-block Crossing	Near Assisted Living to access the beach. Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.
US-90 between Veterans Ave & Rodenberg Ave	Biloxi	Mid-block Crossing	Number of hotels along this area. Large medians offer opportunity for median refuge island. Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.
US-90 between Briarfield Ave & Pine Grove Ave	Biloxi	Mid-block Crossing	Surf Style/Motel 6; bus stop on US-90 EB. PHB should be about 100 ft from nearest roadway, 20 ft beyond free from parking (doable but tight for EB direction). Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.



CITY OF D'IBERVILLE

The City of D'Iberville is recommended to implement mainly bicycle facilities and sidewalks, with a few in key areas. The projects are divided into different phases, with phase one envisioned as near-term or highest priority projects. The highest priority projects are shown in the darkest colors, with lower priority shown in more faded hues. Phase I projects are shown in Figure 30.

Phase I projects for D'Iberville include bicycle facilities and sidewalks along Brodie Road, Meadow Drive, Lamey Bridge Road and Gorenflo Road. Two key trail sections are recommended in Phase I: along the bay on Bay Shore Drive and between Cook Road and Highland Avenue

along Lamey Bridge Road. There are two spot improvements planned for the City of D'Iberville: intersection improvements for pedestrians at D'Iberville Boulevard and Auto Mall Parkway as well as at Rodriguez Street and Brodie Road.

Phase II and Phase III projects are shown in Figures 31-32. Phase II includes trails: sidepaths or greenways, along Popps Ferry, Galleria and parallel to I-110. These trails are extended in Phase III. Phase I also adds bicycle and pedestrian improvements along Central Avenue, D'Iberville Boulevard and Racetrack. Phase II adds sidewalk improvements along Ginger Drive and north of Gorenflo Road.

Figure 30 D'Iberville Phase I Recommendations

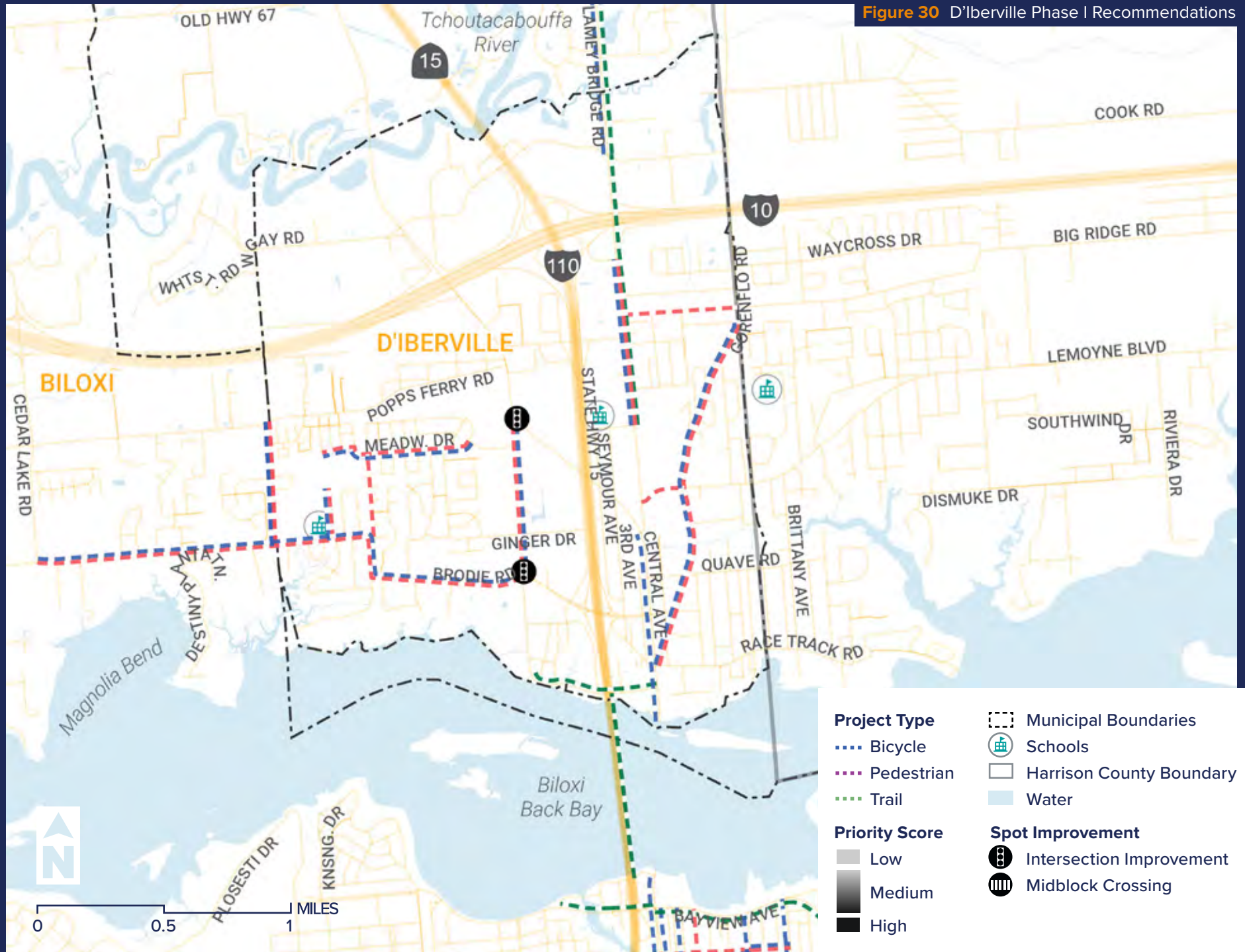


Figure 31 D'Iberville Phase II Recommendations

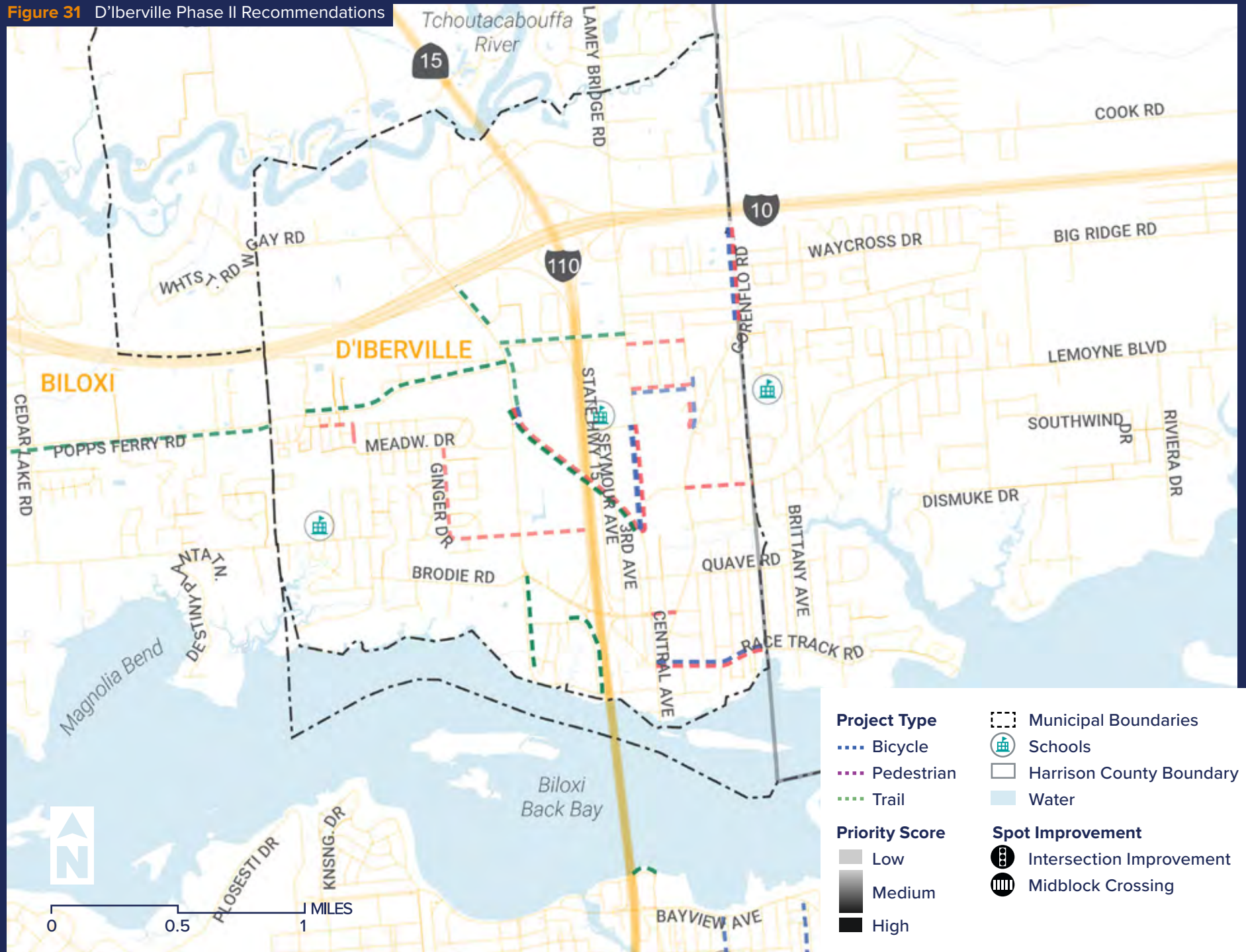
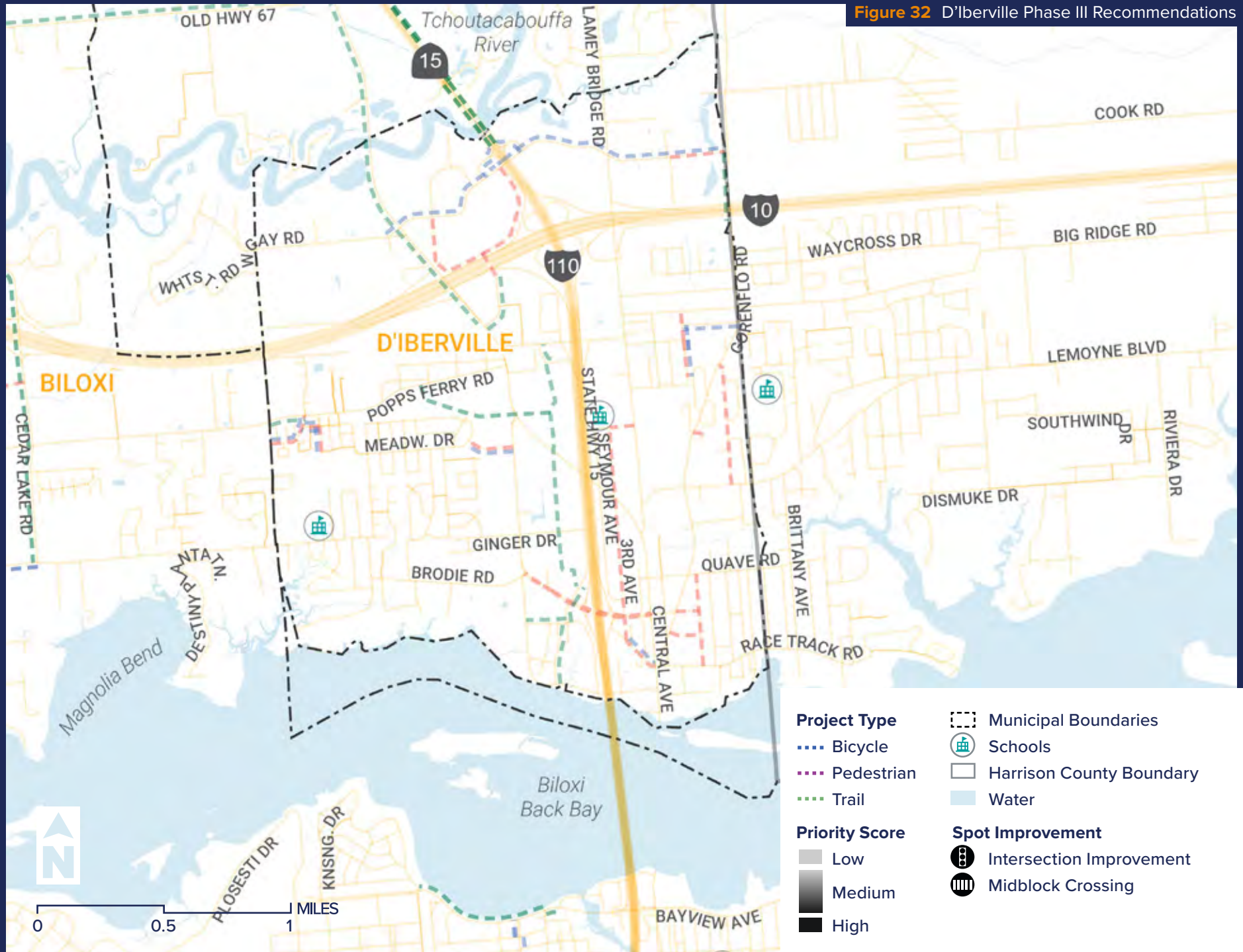


Figure 32 D'Iberville Phase III Recommendations



CITY OF D'IBERVILLE RECOMMENDATIONS

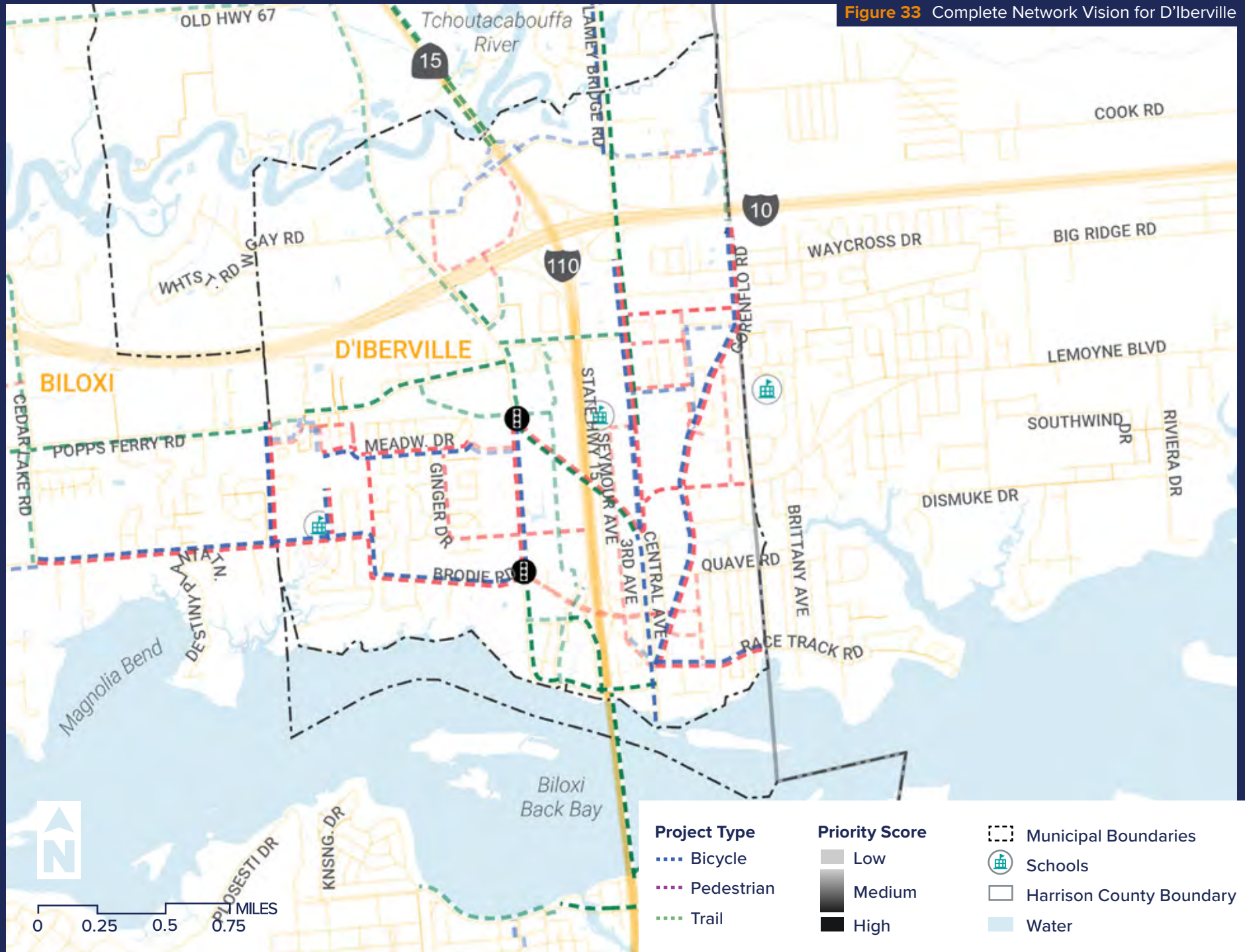
Phase I have prioritization score 0.42 and above

Phase II have prioritization score 0.34 to 0.41

Phase III have prioritization score 0.33 or less

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Auto Mall Pkwy	D'iberville Blvd	Brodie Rd	Pedestrian	0.58	I
Auto Mall Pkwy	D'iberville Blvd	Brodie Rd	Bicycle	0.58	I
Auto Mall Pkwy	Auto Mall Pkwy Undivided	Brodie Rd	Pedestrian	0.05	I
Auto Mall Pkwy	Auto Mall Pkwy Undivided	Brodie Rd	Bicycle	0.05	I
Auto Mall Pkwy	Auto Mall Pkwy Undivided	Brodie Rd	Pedestrian	0.05	I
Auto Mall Pkwy	Auto Mall Pkwy Undivided	Brodie Rd	Bicycle	0.05	I
Kajja Dr	Brodie Rd	Hemlock Dr	Pedestrian	0.22	I
Kajja Dr	Brodie Rd	Hemlock Dr	Bicycle	0.22	I
Lamey Bridge Rd	Warrior Dr	Georgette Ln	Pedestrian	0.71	I
Lamey Bridge Rd	Warrior Dr	Georgette Ln	Bicycle	0.71	I
Lamey Bridge Rd	Warrior Dr	I-10 On Ramp	Trail	0.71	I
Lamey St	Brodie Rd	Popps Ferry Rd	Pedestrian	0.52	I
Lamey St	Brodie Rd	Popps Ferry Rd	Pedestrian	0.52	I
Lamey St	Brodie Rd	Popps Ferry Rd	Bicycle	0.52	I
Lamey St	Brodie Rd	Popps Ferry Rd	Bicycle	0.52	I
Brodie Rd	Cedar Lake Rd	7th Ave	Pedestrian	2.22	I
Brodie Rd	Cedar Lake Rd	7th Ave	Bicycle	2.22	I
Big Ridge Rd	Lamey Bridge Rd	Gorenflo Rd	Pedestrian	0.49	I
Central Ave	Sunset Dr	Lamey Bridge Rd	Bicycle	0.80	I
Lemoyne Blvd	Lamey Bridge Rd	Gorenflo Rd	Pedestrian	0.20	I
Merigold Dr	Atwood Dr	Meadow Dr	Pedestrian	0.35	I
Rolling Height Dr	Merigold Dr	Sparrow Dr	Bicycle	0.21	I
Bay Shore Dr	Central Ave	Moran St	Trail	0.59	I
Lamey Bridge Rd	Georgette Ln	Sangani Blvd	Trail	0.44	I
Gorenflo Rd	Race Track Rd	Chatham Rd	Pedestrian	1.53	I

Figure 33 Complete Network Vision for D'Iberville



CITY OF D'IBERVILLE RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Gorenflo Rd	Race Track Rd	Chatham Rd	Bicycle	1.53	I
Meadow Dr	Suzanne Dr	Sparrow Dr	Pedestrian	0.65	I
Meadow Dr	Suzanne Dr	Sparrow Dr	Bicycle	0.65	I
Auto Mall Pkwy South	D'iberville Blvd	Auto Mall Pkwy	Pedestrian	0.07	II
Auto Mall Mall Pkwy North	D'iberville Blvd	Auto Mall Pkwy	Bicycle	0.07	II
Auto Mall Pkwy Connector North	Auto Mall Pkwy	D'iberville Blvd	Pedestrian	0.04	II
7th Ave	Bay Shore Dr	Brodie Rd	Trail	0.39	II
Boney Ave	Bay Shore Dr	5th Ave	Trail	0.37	II
Gorenflo Rd	Chatham Rd	Interstate 10	Pedestrian	0.42	II
Gorenflo Rd	Chatham Rd	Interstate 10	Bicycle	0.42	II
Race Track Rd	Central Ave	Batia Ave	Pedestrian	0.48	II
Race Track Rd	Central Ave	Batia Ave	Bicycle	0.48	II
Popps Ferry Rd	D'iberville Blvd	Windsor Hill Dr	Trail	0.89	II
Galleria Pkwy	Mandal Pkwy	I-10 Eb Exit 46c	Trail	0.21	II
Lamey Bridge Rd	D'iberville Blvd	Warrior Dr	Pedestrian	0.46	II
Lamey Bridge Rd	D'iberville Blvd	Warrior Dr	Pedestrian	0.46	II
Lamey Bridge Rd	D'iberville Blvd	Warrior Dr	Bicycle	0.46	II
Lamey Bridge Rd	D'iberville Blvd	Warrior Dr	Bicycle	0.46	II
Lemoyne Blvd	Gorenflo Rd	Vicki Dr	Pedestrian	0.29	II
Rodriguez St	Central Ave	Gorenflo Rd	Pedestrian	0.11	II
D'iberville Blvd	Auto Mall Pkwy	Lamey Bridge Rd	Trail	0.77	II
D'iberville Blvd	Auto Mall Pkwy	Lamey Bridge Rd	Trail	0.77	II
D'iberville Blvd	Auto Mall Pkwy	Lamey Bridge Rd	Pedestrian	0.77	II
Rolling Heights Dr	Golden Cir	W. Meadow Dr	Pedestrian	0.06	II
Auto Mall Pkwy	D'iberville Blvd	Big Ridge Rd	Trail	0.25	II
Bachman Rd	Lamey Bridge Rd	Noll Dr	Pedestrian	0.25	II
E Orchard Loop	Peach St	Gorenflo Rd	Pedestrian	0.14	II
E Orchard Loop	Peach St	Gorenflo Rd	Bicycle	0.14	II
Ginger Dr	Meadow Dr	Boney Ave	Pedestrian	0.95	II
Popps Ferry	Lamey Bridge Rd	D'iberville Blvd	Trail	0.53	II
Rolling Heights Dr	Golden Cir	Sparrow Dr	Pedestrian	0.18	II

CITY OF D'IBERVILLE RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Toncrey Rd	Lamey Bridge Rd	Noll Dr	Pedestrian	0.28	II
Toncrey Rd	Noll Dr	Lamey Bridge Rd	Bicycle	0.28	II
Popps Ferry to Utility Greenway	Popps Ferry Rd	Boney Ave Utility Corridor	Trail	0.61	II
Bobby Eleuterius Blvd	D'iberville Blvd	Promenade Pkwy	Pedestrian	0.19	III
Promenade Pkwy	Highway 15	Sangani Blvd	Bicycle	0.13	III
Bachman Rd	Bachman Rd	Big Ridge Rd	Pedestrian	0.12	III
Bachman Rd	Chatham Rd	Bachman Rd	Bicycle	0.06	III
Boney Ave	Rodriguez St	Utility Corridor	Trail	0.08	III
Boney Ave	Wood View Dr	Popps Ferry Rd	Trail	0.10	III
Cardinal Dr	Rolling Hills Pkwy	Sparrow Dr	Bicycle	0.07	III
Chatham Rd	Gorenflo Rd	Noll Dr	Bicycle	0.24	III
Diaz St	Gorenflo Rd	Pringle Ave	Pedestrian	0.14	III
Douglass Dr	Douglas Dr	Douglas Dr	Pedestrian	0.07	III
I-10 Pedestrian Overpass	Bobby Eleuterius Blvd	Mandal Pkwy	Pedestrian	0.12	III
Indian River Rd	Sangani Blvd	Highway 15	Bicycle	0.09	III
Noll Dr	Bachman Rd	Cemetery Rd	Pedestrian	0.16	III
Noll Dr	Bachman Rd	Cemetery Rd	Bicycle	0.16	III
Popps Ferry Rd	Lamey St, Gay Rd	Windsor Hill Dr	Trail	0.15	III
Pringle Ave	Race Track Rd	Quave Rd	Pedestrian	0.39	III
Promenade Pkwy	W. Gay Rd	Bobby Eleuterius Blvd	Bicycle	0.59	III
Promenade Pkwy Off Ramp	Promenade Pkwy	Highway 15	Bicycle	0.05	III
Rodriguez St	Brodie Rd	Central Ave	Pedestrian	1.10	III
Rodriguez St	Gorenflo Rd	Pringle Ave	Pedestrian	0.10	III
Rolling Heights Dr	Sparrow Dr	Rolling Hills Pkwy	Pedestrian	0.07	III
Rolling Heights Dr	Sparrow Dr	Rolling Hills Pkwy	Bicycle	0.07	III
Sangani Blvd	Bobby Eleuterius Blvd	Highway 15	Bicycle	0.03	III
Sangani Blvd	Highway 15	Lamey Bridge Rd	Bicycle	0.48	III
Sparrow Dr	W. Meadow Dr	Cardinal Dr	Pedestrian	0.11	III
Sparrow Dr	W. Meadow Dr	Cardinal Dr	Bicycle	0.11	III
Suzanne Dr	Auto Mall Pkwy	Meadow Dr	Bicycle	0.20	III
Suzanne Dr	Auto Mall Pkwy	Meadow Dr	Pedestrian	0.20	III
Voll Dr	Bachman Rd	375 Ft South Of Bachman Dr	Bicycle	0.07	III

CITY OF D'IBERVILLE RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Old Hwy 15	Old Highway 67	Mcalpine St	Trail	1.57	III
Bobby Eleuterius Blvd	Sangani Blvd	Southwest Corner Of Mall	Pedestrian	0.69	III
Daisy Vestry Rd	Mallett Rd	Interstate 10	Trail	0.29	III
Mallett Rd	Lamey Bridge Rd	Daisy Vestry Rd	Bicycle	0.50	III
Octave St	Lemoyne Blvd	Gorenflo Rd	Pedestrian	0.51	III
Rolling Hills Pkwy	Lamey St	Popps Ferry Rd	Pedestrian	0.22	III
Rolling Hills Pkwy	Lamey St	Popps Ferry Rd	Bicycle	0.22	III
W Race Track Rd	3rd Ave	Central Ave	Bicycle	0.17	III
W Race Track Rd	Central Ave	3rd Ave	Pedestrian	0.17	III
Warrior Dr	Lamey Bridge Rd	3rd Ave	Pedestrian	0.11	III
Mallett Rd	Cinema Dr	Daisy Vestry Rd	Pedestrian	0.23	III
3rd Ave	W. Race Track Rd	Warrior Dr	Pedestrian	0.90	III
Boney Ave Utility Corridor	Wood View Dr	Boney Ave	Trail	1.01	III
5th Ave	Rodriguez St	Talley St	Trail	0.09	III
Santa Cruz Ave	Bay Shore Dr	Talley St	Trail	0.26	III
Talley St	5th Ave	7th Ave	Trail	0.17	III
Highway 15 Connector	Highway 15	Sangani Blvd, Highway 15	Bicycle	0.05	III
Mandal Pkwy	D'iberville Blvd	Sb Off Ramp 46b	Trail	0.24	III

SPOT IMPROVEMENTS

The following table outlines key spot improvements for the City of D'Iberville.

LOCATION	CITY	TYPE	DESCRIPTION
D'Iberville Blvd and Auto Mall Pkwy	D'Iberville	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
Rodriguez St and Brodie Rd	D'Iberville	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.



CITY OF GULFPORT

The City of Gulfport has already invested in sidewalks and some multi-use paths useful for active living. The Initiative recommends Gulfport to add more sidewalks, trails and bicycle facilities in phases. Phase I is envisioned as near-term or highest priority projects. The highest priority projects are also shown in the darkest colors, with lower priority shown in more faded hues.

As shown in Figure 34 Gulfport is recommended in Phase I to add sidewalks or bicycle facilities such as bike lanes or sidepaths near the coast, and trails such as greenways further inland. Figure 35 and Figure 36 provide a closer look at the Phase I recommendations along the coastline in Gulfport. Bicycle facilities are recommended east-west along Outpass Road, West Railroad Street, 17th Street, 25th Street, East Railroad Street, West Pine Street and South Railroad Street. Bicycle facilities are recommended north-south along Broad Avenue, 42nd Avenue, 30th Avenue, US-49, 20th Avenue, Thornton Avenue, Hewes Avenue and Cowan Road. Sidewalks are recommended throughout neighborhoods between Lewis Avenue

and 42nd Avenue, as well as along Railroad Street, Hewes Avenue, Tegarden Road, and West Pine Street.

Gulfport should next prioritize trails along US-90 / Beach Boulevard, East Railroad Street, Airport Road and Dedeux Road, as shown in Figure 37. Additional gaps in pedestrian and bicycle facilities will be filled, including some key connections to schools in the northern area of the city. Some of the key north-south bicycle connections near the coast include: F avenue, Washington Avenue, Hancock Avenue, Courthouse Road and Anniston Avenue. These recommendations as a well as other Phase II projects are shown in Figure 38 and Figure 39.

Phase III projects in Gulfport connect Airport Road and Rippy Road with local schools, add trails along Lorrain Road, Oneal Road and parallel to US-49, and fill in additional sidewalk gaps. Near the coast, sidewalks and bicycle facilities make up the majority of Phase III projects, with a couple of trails included on the east side of the city. See Figures 40-42.

Figure 34 Gulfport Phase I Recommendations

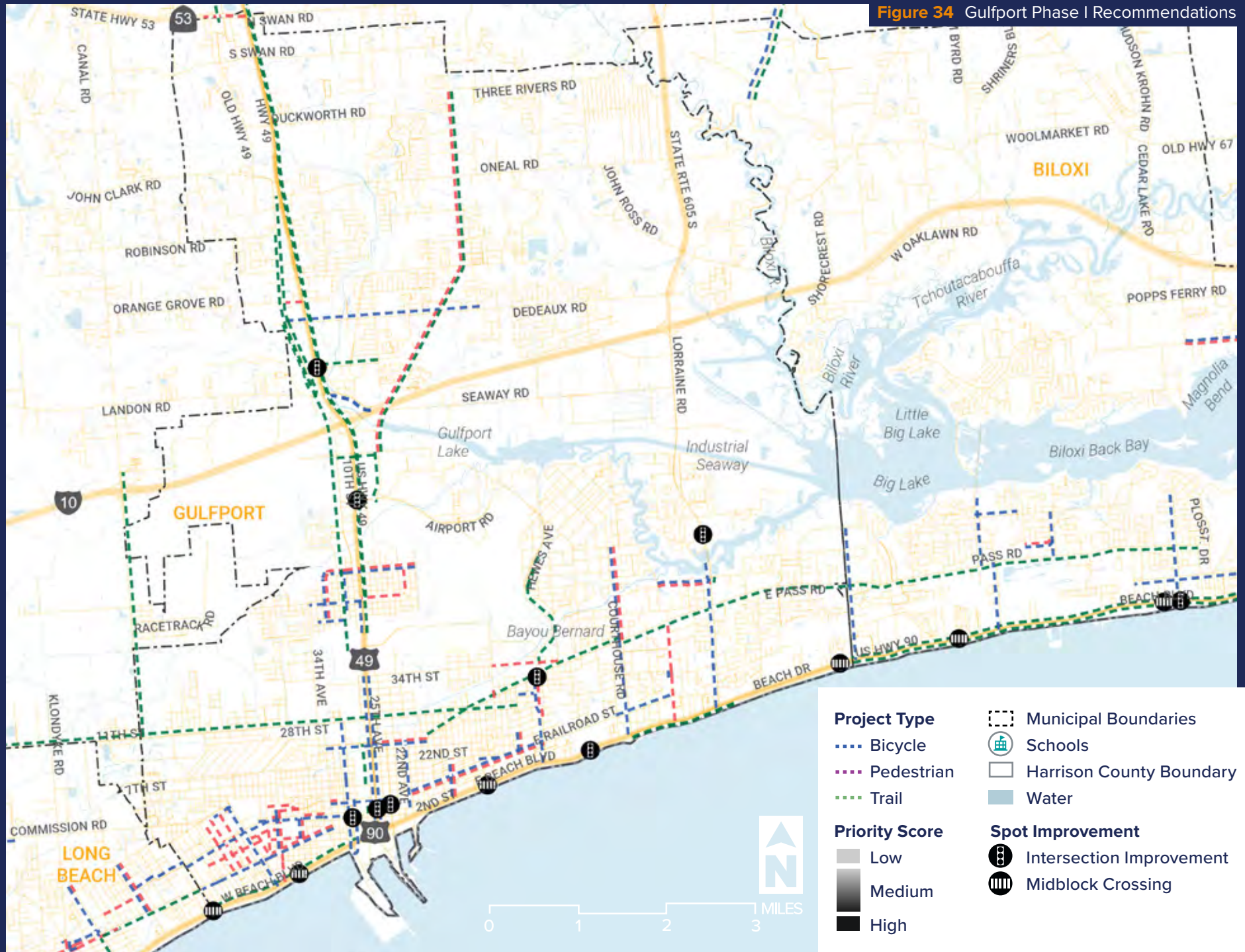


Figure 35 Downtown Gulfport Phase I Recommendations



Figure 36 Coastal East Gulfport Phase I Recommendations

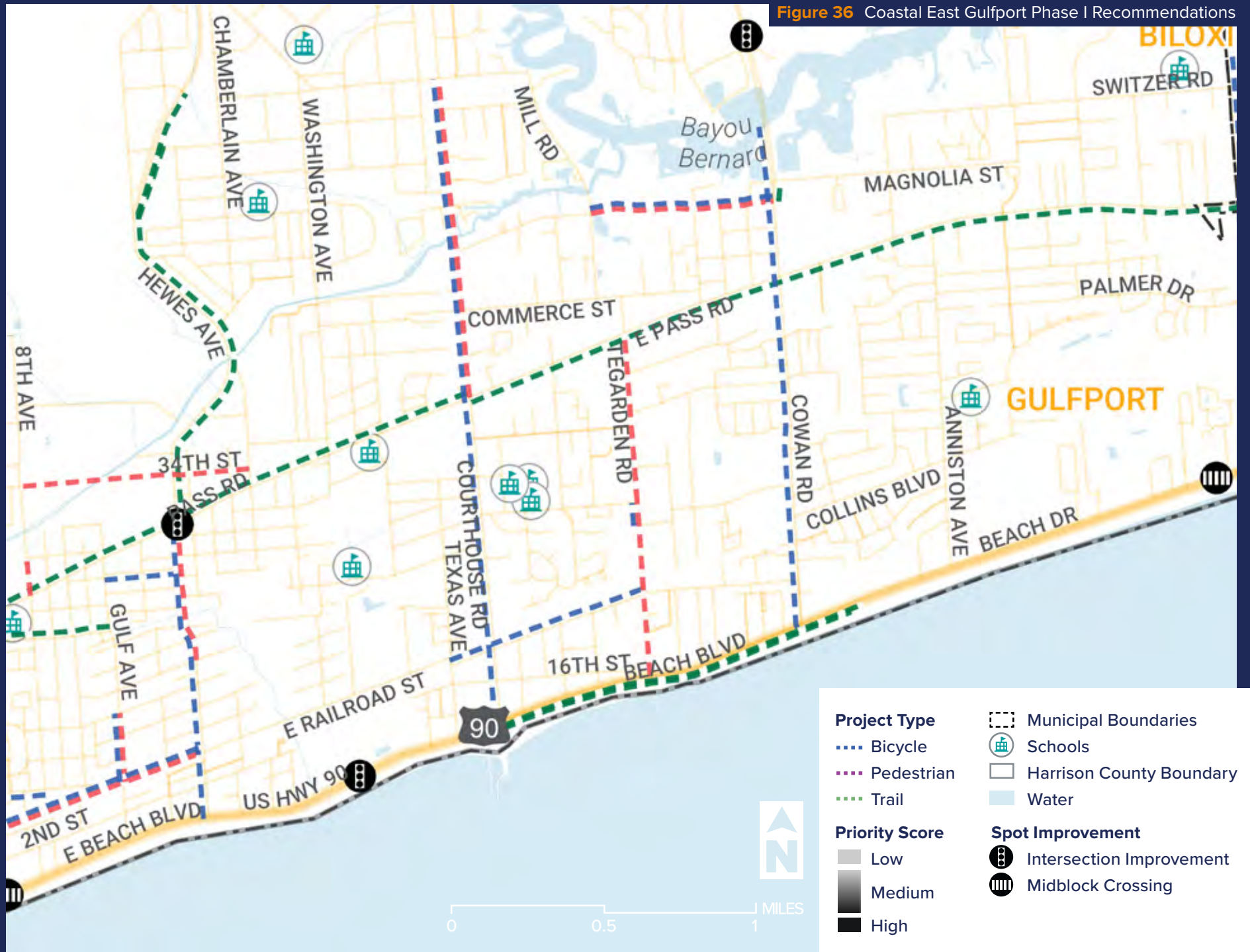


Figure 37 Gulfport Phase II Recommendations

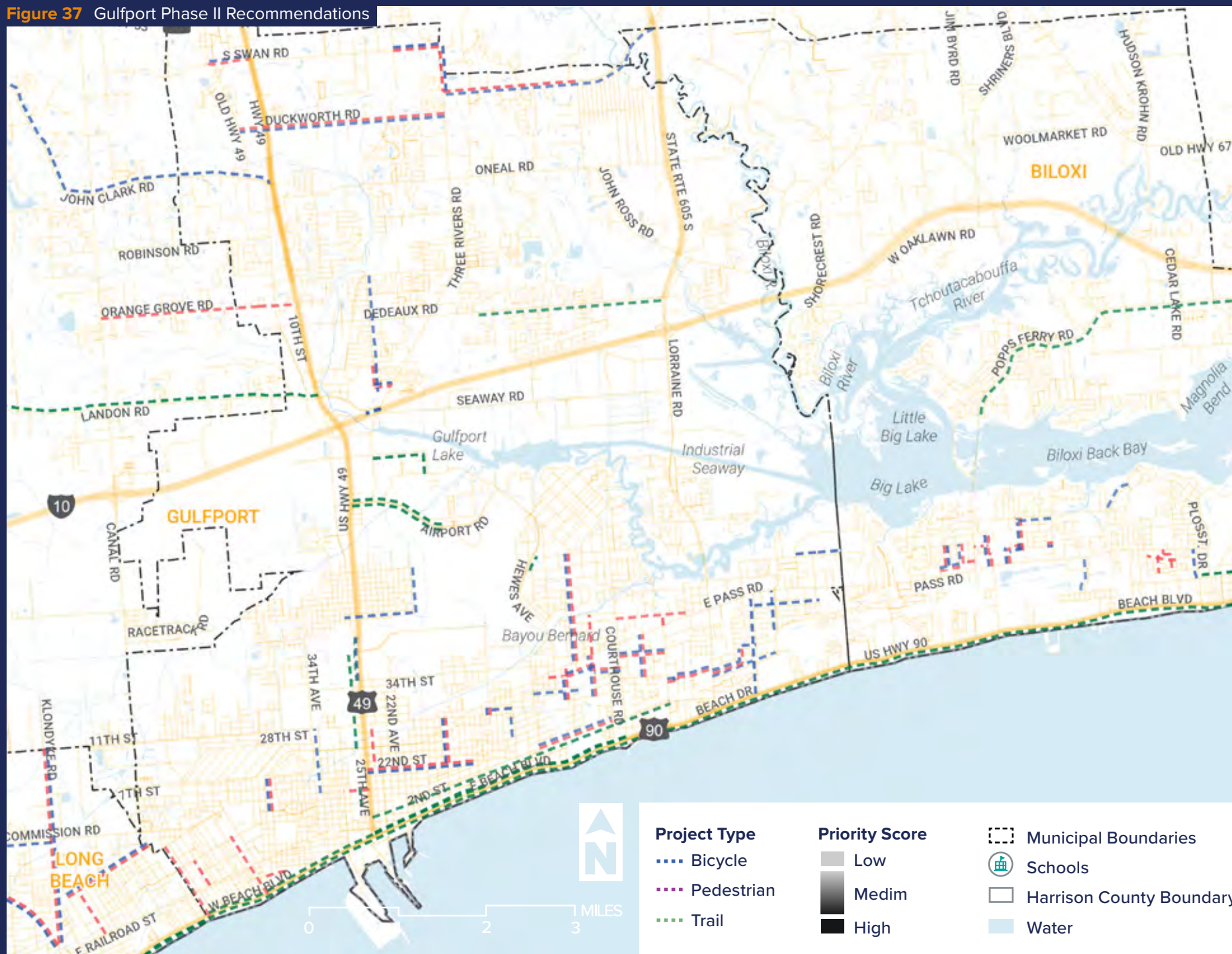


Figure 38 Downtown Gulfport Phase II Recommendations



Figure 39 Coastal East Gulfport Phase II Recommendations



Figure 40 Gulfport Phase III Recommendations

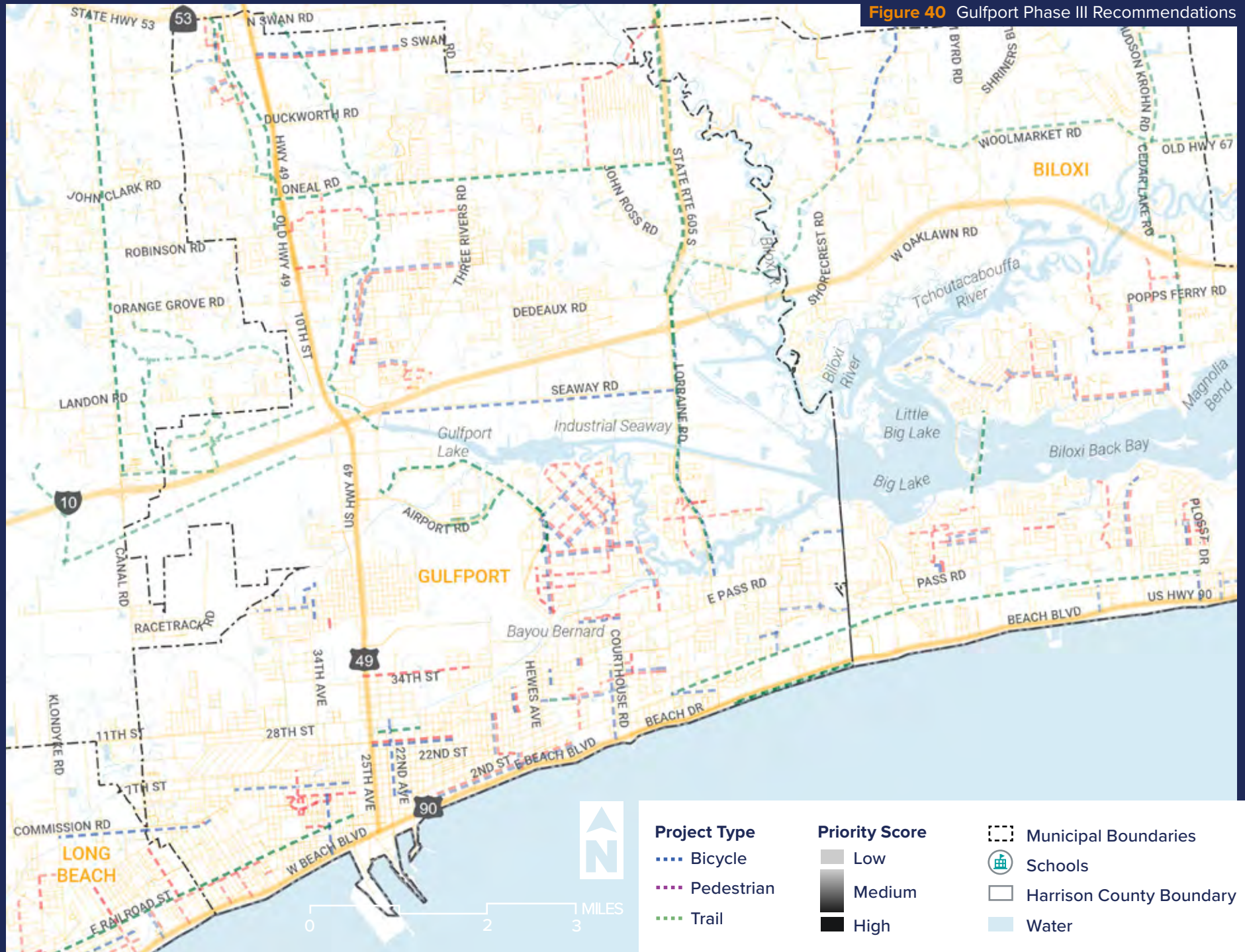


Figure 41 Downtown Gulfport Phase III Recommendations

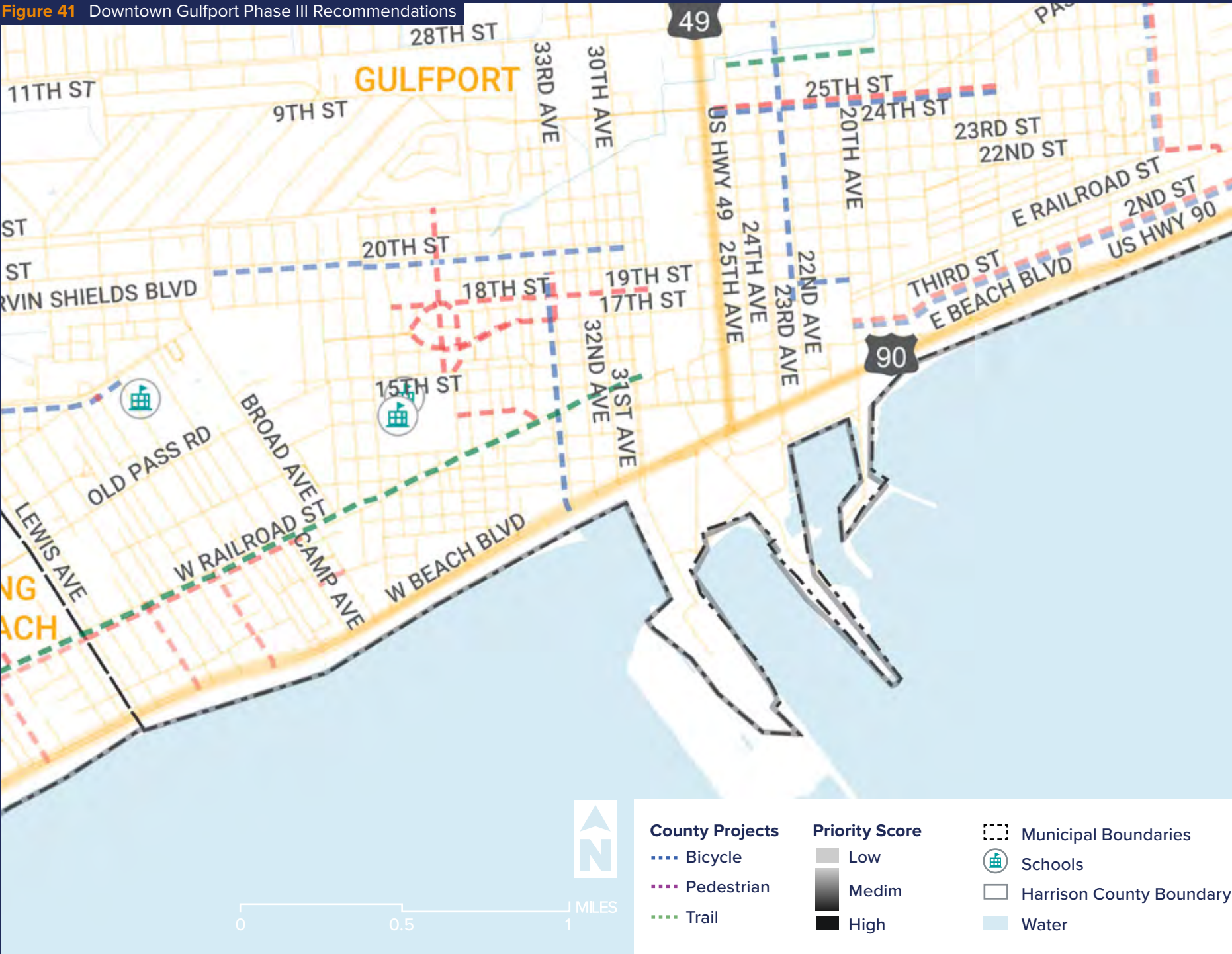
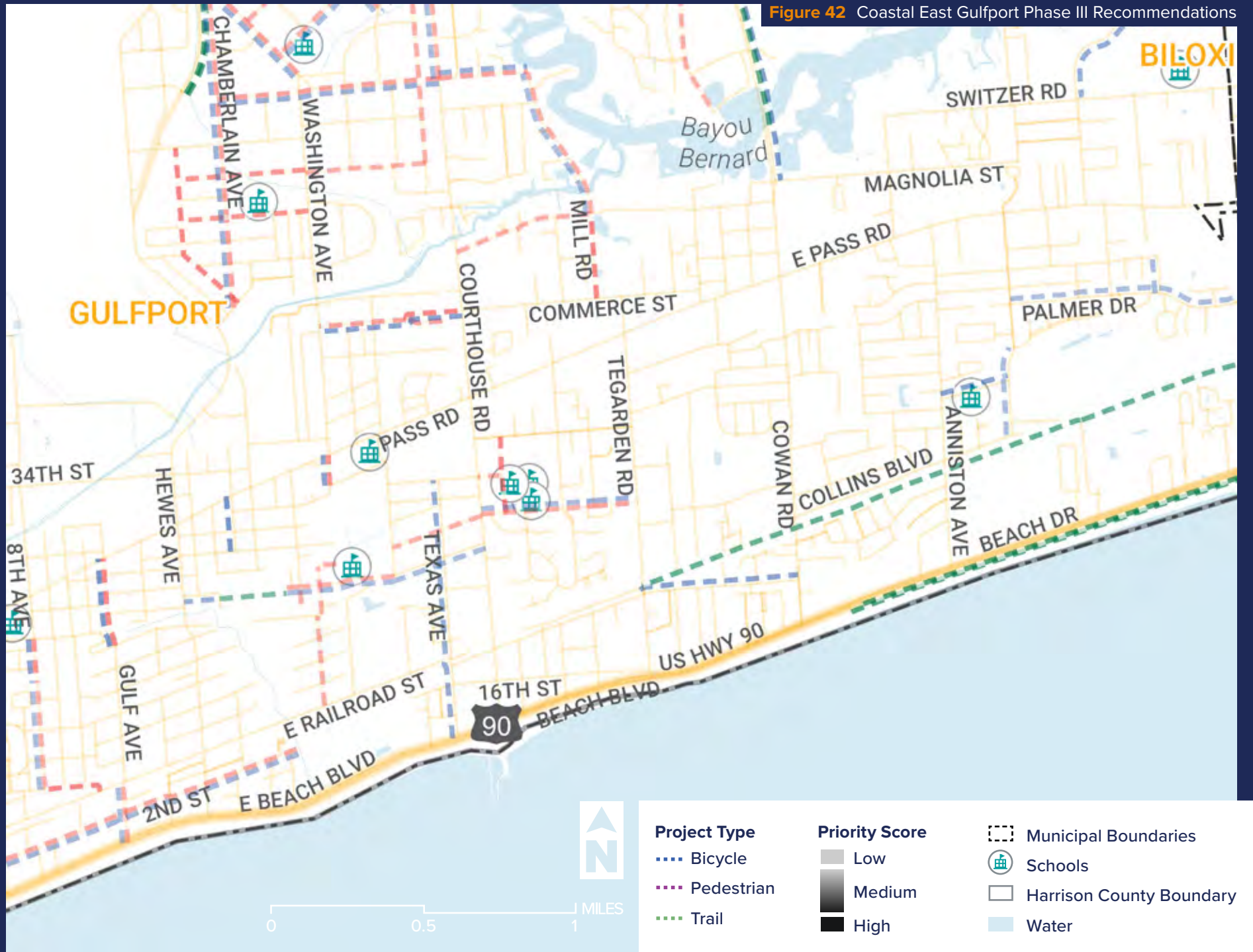


Figure 42 Coastal East Gulfport Phase III Recommendations



CITY OF GULFPORT RECOMMENDATIONS

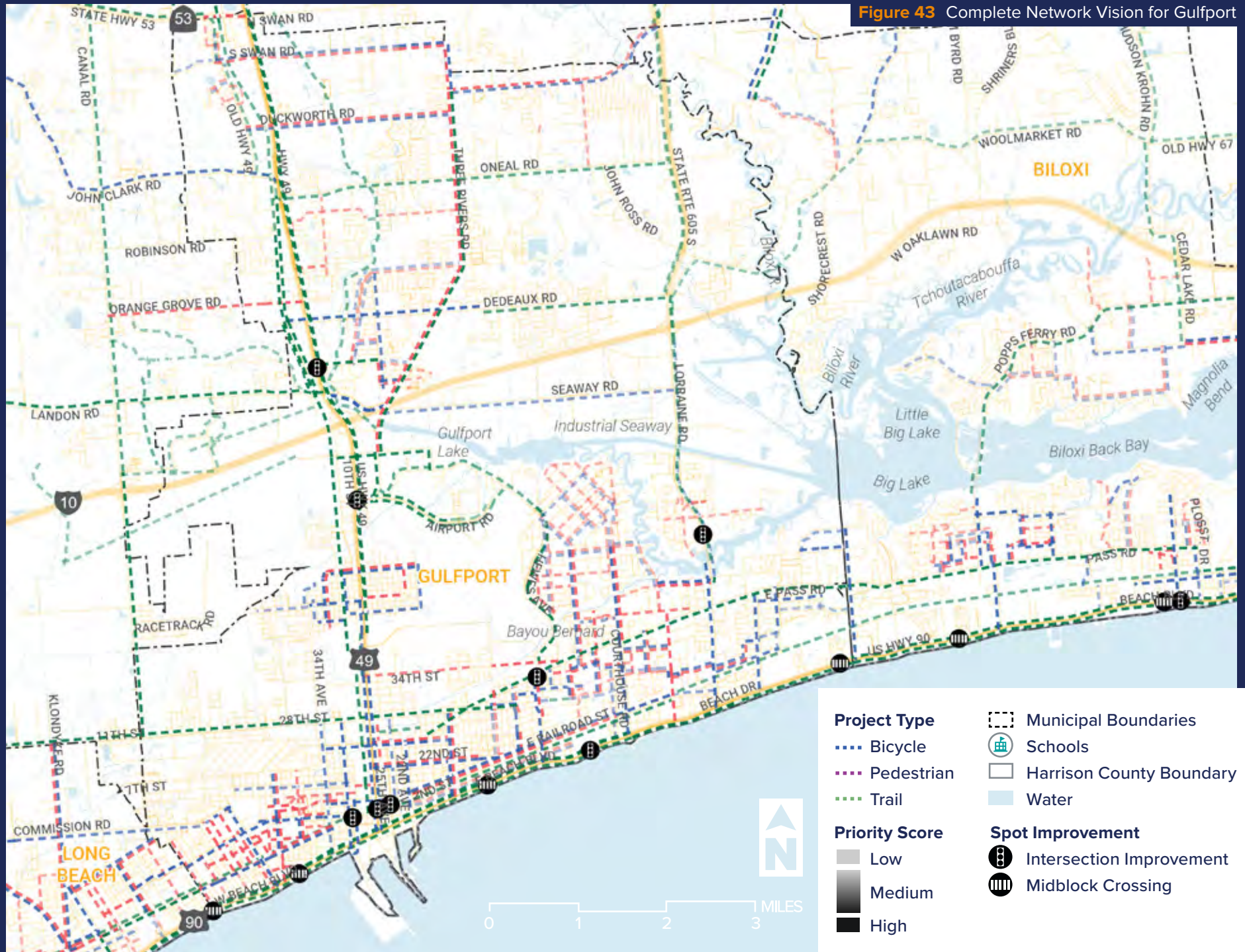
Phase I have prioritization score 0.42 and above

Phase II have prioritization score 0.34 to 0.41

Phase III have prioritization score 0.33 or less

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
15th St	42nd Ave	44th Ave	Pedestrian	0.14	I
15th St	42nd Ave	44th Ave	Bicycle	0.14	I
Creosote Rd	Arkansas Ave	Highway 49	Trail	0.20	I
Broad Ave	W. Beach Blvd	Engram Dr	Bicycle	1.08	I
15th St	44th Ave	Canon Hospice (Western Street Terminus)	Bicycle	0.11	I
42nd Ave	8th St	W. Railroad St	Pedestrian	0.14	I
42nd Ave	W. Railroad St	15th St	Pedestrian	0.35	I
42nd Ave	W. Railroad St	15th St	Bicycle	0.35	I
Old Pass Rd	44th Ave	Lewis Ave	Bicycle	1.08	I
44th Ave	W Railroad St	13th St	Pedestrian	0.27	I
45th Ave	W Railroad St	13th St	Pedestrian	0.25	I
Creosote Rd	Highway 49	Three Rivers Rd	Trail	0.27	I
Hewes Ave	26th St	Pass Rd	Pedestrian	0.45	I
Martin Luther King Jr Blvd	Highway 49	34th Ave	Bicycle	0.61	I
Old Magnolia Rd	W. Dedeaux Rd	Richmar Dr (Sam's Club)	Trail	0.76	I
W Railroad St	33rd Ave	44th Ave	Pedestrian	0.77	I
W Railroad St	33rd Ave	44th Ave	Bicycle	0.77	I
Woodward Ave	W. Railroad St	Engram Dr	Pedestrian	0.69	I
Old Hwy 49	Landon Rd	Dedeaux Rd	Trail	0.88	I
13th St	42nd Ave	Broad Ave	Pedestrian	0.35	I
15th St	38th Ave	42nd Ave	Bicycle	0.25	I
Martin Luther King Jr Blvd	South Carolina Ave	Highway 49	Pedestrian	0.43	I
Pass Rd	Courthouse Rd	28th St	Trail	1.94	I
W Railroad St	44th Ave	Ruth Ave	Bicycle	0.62	I

Figure 43 Complete Network Vision for Gulfport



CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
E Pass Rd	Courthouse Rd	Debuys Rd	Trail	2.64	I
E Railroad St	20th Ave	Hewes Ave	Pedestrian	1.60	I
E Railroad St	20th Ave	Hewes Ave	Bicycle	1.60	I
30th Ave	20th St	28th St	Bicycle	0.65	I
Crossroads Pkwy WB	Crossroads Pkwy	Highway 49	Bicycle	0.57	I
Crossroads Pkwy EB	Crossroads Pkwy	Highway 49	Bicycle	0.57	I
Thornton Ave	E. Beach Blvd	22nd St	Pedestrian	0.43	I
Thornton Ave	E. Beach Blvd	22nd St	Bicycle	0.43	I
Cowan Rd	W. Pine St	Lorraine Rd Bridge	Bicycle	0.25	I
Cowan Rd	W. Pine St	Magnolia St	Trail	0.07	I
Engram Dr	Broad Ave	Mills Ave	Bicycle	0.27	I
Fournier Ave	W. Railroad St	Old Pass Rd	Pedestrian	0.35	I
Three Rivers Rd	Creosote Rd	Airport Rd	Trail	0.53	I
17th St	22nd Ave	32nd Ave	Bicycle	0.69	I
W Pine St	Mill Rd	Cowan Rd	Pedestrian	0.60	I
W Pine St	Mill Rd	Cowan Rd	Bicycle	0.60	I
Hewes Ave	E. Beach Blvd	Pass Rd	Bicycle	0.97	I
10th St	Arkansas Ave	Martin Luther King Jr. Blvd	Bicycle	0.40	I
12th St	38th Ave	44th Ave	Pedestrian	0.39	I
25th Ave	34th St	Beach Blvd	Bicycle	1.76	I
25th Ave	Beach Blvd	34th St	Bicycle	1.75	I
25th St	25th Ave	33rd Ave	Bicycle	0.51	I
30th Ave	17th St	20th St	Bicycle	0.22	I
42nd Ave	W. Beach Blvd	8th St	Pedestrian	0.23	I
8th Ave	Pass Rd	31st St	Pedestrian	0.12	I
E Railroad Ave	Madison St	Polk St	Bicycle	0.46	I
Hewes Ave	South Of Bayou View Sports Complex Parking Lot	47th St	Trail	0.26	I
Jefferson St	West Railroad Ave	Michigan Ave	Bicycle	0.32	I
Kansas City Southern Railroad	W. Railroad St	17th St	Trail	0.13	I
Polk St	East Railroad Ave	Illinois Ave	Pedestrian	0.24	I

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Polk St	East Railroad Ave	Illinois Ave	Bicycle	0.24	I
Tegarden Rd	Beach Blvd	Silver Ln	Pedestrian	0.49	I
Cowan Rd	Beach Blvd	W. Pine St	Bicycle	1.40	I
Mills Ave	W. Railroad St	Engram Dr	Pedestrian	0.68	I
Beach Blvd	East Of Cowan Rd	Courthouse Rd	Trail	1.26	I
Beach Blvd	East Of Cowan Rd	Courthouse Rd	Trail	1.22	I
Tegarden Rd	White St	E. Pass Rd	Pedestrian	0.56	I
Hewes Ave	Pass Rd	Bayou View Sports Complex	Trail	1.20	I
11th St	45th Ave	Hardy Ave	Pedestrian	0.66	I
34th St	Jody Nelson Dr	8th Ave	Pedestrian	0.75	I
Community Rd	Klein Rd	Highway 49	Trail	0.69	I
Kansas City Southern Railroad	Creosote Rd	John Hill Blvd	Trail	2.18	I
School Connector	North Gulfport 7th Grade School	Polk St	Pedestrian	0.06	I
School Connector	North Gulfport 7th Grade School	Polk St	Bicycle	0.06	I
16th St	Terrace Dr	42nd Ave	Pedestrian	0.22	I
42nd Ave	15th St	17th St	Pedestrian	0.13	I
42nd Ave	15th St	17th St	Bicycle	0.13	I
Genevieve Dr	Old Pass Rd	Jo Ellen Cir	Pedestrian	0.28	I
North Carolina Ave	Jackson St	Polk St	Pedestrian	0.27	I
Park Blvd	Park Blvd	42nd Ave	Pedestrian	0.12	I
Pass Rd	28th St	25th St	Trail	0.54	I
Tegarden Rd	Silver Ln	White St	Pedestrian	0.08	I
Tegarden Rd	Silver Ln	White St	Bicycle	0.08	I
Polk St	Virginia Ave	East Railroad Ave	Pedestrian	0.78	I
Polk St	Virginia Ave	East Railroad Ave	Bicycle	0.78	I
Kansas City Southern Railroad	Robinson Rd	Factory Shops Blvd	Trail	2.61	I
Old Hwy 49	W. Dedeaux Rd	O'Neal Rd	Trail	1.45	I
Three Rivers Rd	Creosote Rd	S. Swan Rd	Pedestrian	4.31	I
Three Rivers Rd	Creosote Rd	S. Swan Rd	Trail	4.31	I
20th Ave	15th St	Pass Rd	Bicycle	0.76	I
Dedeaux Rd	Old Highway 49	Dede Dr	Bicycle	2.23	I

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
28th St	Gulf Ave	Pass Rd	Trail	0.58	I
Hewes Ave	Bayou View Complex Parking Lot Entrance	Mississippi Air National Guard Parking Lot Entrance	Trail	0.45	I
Kansas City Southern Railroad	19th St	Pass Rd	Trail	0.45	I
Pass Rd	25th St	25th Ave	Pedestrian	0.62	I
Pass Rd	25th St	25th Ave	Bicycle	0.62	I
30th St	0.1 Mile South of US 90	17th St	Bicycle	1.09	I
S Railroad St	Texas Ave	Tegarden Rd	Bicycle	0.67	I
15th St	30th Ave	34th Ave	Bicycle	0.32	I
15th St	34th Ave	38th Ave, Westward Dr	Pedestrian	0.25	I
15th St	34th Ave	38th Ave, Westward Dr	Bicycle	0.25	I
17th St	32nd Ave	33rd Ave	Pedestrian	0.08	I
17th St	32nd Ave	33rd Ave	Bicycle	0.08	I
17th St	Terrace Dr	44th Ave	Pedestrian	0.38	I
22nd St	Cypress Ave	Pine Ave	Bicycle	0.23	I
28th St	Pass Rd	Red Creek Rd	Trail	7.24	I
38th Ave	W. Railroad St	15th St	Bicycle	0.26	I
38th Ave	W. Railroad St	15th St,	Pedestrian	0.26	I
Courthouse Rd	E. Beach Blvd	E. Pass Rd	Bicycle	1.07	I
Courthouse Rd	E. Pass Rd	Kahler St, 48th St	Pedestrian	1.05	I
Courthouse Rd	E. Pass Rd	Kahler St, 48th St	Bicycle	1.05	I
Gulf Ave	E. Railroad St	25th St	Pedestrian	0.23	I
Gulf Ave	E. Railroad St	25th St	Bicycle	0.23	I
Illinois Ave	Jefferson St	Tyler St	Pedestrian	0.45	I
Illinois Ave	Jefferson St	Tyler St	Bicycle	0.45	I
Mississippi St	Hewes Ave	Gulf Ave	Bicycle	0.25	I
Orange Grove Blvd	Old Highway 49	Highway 49	Pedestrian	0.22	I
Railroad St	Ruth Ave	Hardy Ave	Bicycle	0.06	I
W Beach Blvd	25th Ave	Lewis Ave	Trail	2.09	I
W Railroad St	Hardy Ave	Lewis Ave	Bicycle	0.19	I
22nd St	Pine Ave	25th Ave	Pedestrian	1.19	II

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
22nd St	Pine Ave	25th Ave	Bicycle	1.19	II
13th Ave	22nd St	28th St	Pedestrian	0.51	II
13th Ave	22nd St	28th St	Bicycle	0.51	II
E Beach Blvd	Courthouse Rd	25th Ave	Trail	3.06	II
S Swan Rd	N. Swan Rd	Three Rivers Rd	Pedestrian	1.02	II
S Swan Rd	N. Swan Rd	Three Rivers Rd	Bicycle	1.02	II
E Beach Blvd	Courthouse Rd	25th Ave	Trail	3.06	II
Creosote Rd	Three Rivers Rd	Rippy Rd	Trail	0.85	II
Airport Rd	Airport Rd, Werby Rd	Highway 49	Trail	1.08	II
Airport Rd	Highway 49	Werby Rd	Trail	1.08	II
Anniston Ave	Service Dr	E. Pass Rd	Bicycle	1.12	II
19th St	25th Ave	30th Ave	Pedestrian	0.28	II
24th Ave	23rd St	28th St	Pedestrian	0.42	II
29th St	Gulf Ave	8th Ave	Bicycle	0.25	II
32nd 1/2 St	C Ave	F Ave	Pedestrian	0.20	II
32nd 1/2 St	F Ave	Maples Dr	Pedestrian	0.15	II
32nd 1/2 St	F Ave	Maples Dr	Bicycle	0.15	II
42nd Ave	17th St	22nd St	Pedestrian	0.38	II
42nd Ave	17th St	22nd St	Bicycle	0.38	II
8th Ave	28th St	Pass Rd	Pedestrian	0.14	II
8th Ave	28th St	Pass Rd	Bicycle	0.14	II
Central St	41st Ave	Broad Ave	Pedestrian	0.23	II
Crossroads Pkwy WB	Three Rivers Rd	972 Ft West Of Three Rivers Rd	Bicycle	0.18	II
Crossroads Pkwy EB	Three Rivers Rd	972 Ft West Of Three Rivers Rd	Bicycle	0.19	II
Demaret Dr	Anniston Ave	Hayden Dr	Bicycle	0.73	II
F Ave	29th St	32nd-1/2 St	Pedestrian	0.27	II
F Ave	29th St	32nd-1/2 St	Bicycle	0.27	II
F Ave	32nd-1/2 St	Pass Rd	Pedestrian	0.12	II
Ford Ave	Greenview Dr	E. Pass Rd	Bicycle	0.44	II
Hancock Ave	Pass Rd	Canal St	Pedestrian	0.45	II
Hardy Ave	W. Railroad St	Old Pass Rd	Pedestrian	0.33	II

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Hewes Ave	47th St	Hewes Ave	Trail	0.19	II
Kansas City Southern Railroad	17th St	19th St	Trail	0.20	II
Klein Rd	Community Rd	River Ten Rd	Pedestrian	0.24	II
Klein Rd	Community Rd	River Ten Rd	Bicycle	0.24	II
Klein Rd	River Ten Rd	River Ten Rd	Bicycle	0.05	II
Odonnell Blvd	St. James Blvd	Anniston Ave	Bicycle	0.16	II
River Ten Rd	Klein Rd	Three Rivers Rd	Pedestrian	0.20	II
River Ten Rd	Klein Rd	Three Rivers Rd	Bicycle	0.20	II
Switzer Rd	300 Ft East Of Oakleigh Ave	Debuys Rd	Bicycle	0.26	II
Switzer Rd	Runnymede Dr	300 Ft East Of Oakleigh Ave	Bicycle	0.25	II
Tegarden Rd	E. Pass Rd	Commerce St	Pedestrian	0.12	II
Three Rivers Rd	S. Swan Rd	John Rd	Pedestrian	1.51	II
Us Hwy 90	Jackson County	Hancock County	Trail	29.63	II
Madison St	Virginia Ave	East Railroad Ave	Bicycle	0.77	II
Washington Ave	Pass Rd	48th St	Pedestrian	1.26	II
Washington Ave	Pass Rd, H Ave	48th St	Bicycle	1.26	II
Kansas City Southern Railroad	Pass Rd	West Of John Hill Blvd	Trail	1.05	II
Orange Grove Rd	250 Ft East Of Brookfield Dr	Highway 49	Pedestrian	0.43	II
W Beach Blvd	25th Ave	Lewis Ave	Trail	2.08	II
Camp Ave	W. Beach Blvd	Old Pass Rd	Pedestrian	0.77	II
Csxt L And N Railroad	25th Ave	Tegarden Rd	Trail	3.58	II
25th Ave	34th St	Arkansas Ave	Bicycle	0.48	II
25th Ave	Arkansas Ave	34th St	Trail	0.48	II
31st St	F Ave	Hewes Ave	Bicycle	0.51	II
33rd Ave	19th St	28th St	Bicycle	0.72	II
33rd St	Courthouse Rd	Hancock Ave	Pedestrian	0.30	II
33rd St	Courthouse Rd	Hancock Ave	Bicycle	0.30	II
5th St	Live Oak Ave	Cowan Rd	Pedestrian	0.41	II
5th St	Live Oak Ave	Cowan Rd	Bicycle	0.41	II
Collins Blvd	Cowan Rd	Sarazen Dr	Bicycle	0.76	II
Gulf Ave	25th St	27th St	Bicycle	0.22	II

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Hancock Ave	30th St	Pass Rd	Bicycle	0.47	II
Hancock Ave	30th St, 29th St	Pass Rd	Pedestrian	0.47	II
Klein Rd	Wilson Blvd	Community Rd	Bicycle	1.01	II
Live Oak Ave	Silver Ln	5th St	Pedestrian	0.04	II
Live Oak Ave	Silver Ln	5th St	Bicycle	0.04	II
Loposser Ave	Perry St	E. Pass Rd	Pedestrian	0.48	II
Loposser Ave	Perry St	E. Pass Rd	Bicycle	0.48	II
Maples Dr	Hancock Ave	H Ave	Pedestrian	0.20	II
Maples Dr	Hancock Ave	H Ave	Bicycle	0.20	II
Saint James Blvd	Cowan Rd	O'donnell Blvd	Pedestrian	0.51	II
Saint James Blvd	Cowan Rd	O'donnell Blvd	Bicycle	0.51	II
Silver Ln	Tegarden Rd	Live Oak Ave	Pedestrian	0.11	II
Silver Ln	Tegarden Rd	Live Oak Ave	Bicycle	0.11	II
Victory Rd	Courthouse Rd	Tegarden Rd	Pedestrian	0.52	II
Victory Rd	Courthouse Rd	Tegarden Rd	Bicycle	0.52	II
Virginia Ave	Polk St	Wilkes St	Bicycle	0.53	II
W Railroad St	Lewis Ave	E. Railroad St	Trail	0.03	II
Duckworth Rd	Three Rivers Rd	Old Highway 49	Pedestrian	2.31	II
Duckworth Rd	Three Rivers Rd	Old Highway 49	Bicycle	2.31	II
Lewis Ave	W. Beach Blvd	17th St	Pedestrian	0.92	II
E Railroad St	Hewes Ave	Texas Ave	Pedestrian	0.87	II
E Railroad St	Hewes Ave	Texas Ave	Bicycle	0.87	II
Three Rivers Rd	Highway 605	S. Swan Rd	Bicycle	2.63	II
S Swan Rd	Old Highway 49	Highway 49	Pedestrian	0.47	II
S Swan Rd	Old Highway 49	Highway 49	Bicycle	0.47	II
Dedeaux Rd	Dede Dr	Lorraine Rd	Trail	2.11	II
Commerce St	Courthouse Rd	Washington Ave	Pedestrian	0.79	II
25th St	13th Ave	25th Ave	Pedestrian	0.91	II
25th St	13th Ave	25th Ave	Bicycle	0.91	II
34th St	11th Ave	26th Ave	Pedestrian	1.26	III
13th St	W. Railroad St	38th Ave	Pedestrian	0.26	III

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
17th St	33rd Ave	Terrace Dr	Pedestrian	0.37	III
18th St	20th Ave	22nd Ave	Bicycle	0.16	III
18th St	29th Ave	41st Ave	Pedestrian	0.81	III
22nd Ave	17th St	Brickyard Bayou Bridge	Bicycle	0.78	III
22nd St	E. Railroad St	Cypress Ave	Pedestrian	0.20	III
33rd Ave	17th St	19th St	Pedestrian	0.15	III
33rd Ave	17th St	19th St	Bicycle	0.15	III
33rd Ave	W. Beach Blvd	17th St	Bicycle	0.60	III
34th Ave	34th Ave	North Gulfport 7th Grade School	Bicycle	0.35	III
38th Ave	18th St	22nd St	Pedestrian	0.30	III
C Ave	31st St	Pass Rd	Bicycle	0.18	III
Commerce St	40th St	Courthouse Rd	Pedestrian	0.20	III
E Beach Blvd	Debuys Rd	Cowan Rd	Trail	1.42	III
E Beach Blvd	Debuys Rd	Cowan Rd	Trail	1.38	III
Gulf Ave	27th St	Pass Rd	Pedestrian	0.29	III
Gulf Ave	27th St	Pass Rd	Bicycle	0.29	III
Hewes Ave	West Of 48th St	54th St	Trail	0.43	III
Hewes Ave	West Of 48th St	54th St	Trail	0.43	III
High School Dr	Perry St	Victory St	Pedestrian	0.25	III
Illinois Ave	Adams St	Adams St	Pedestrian	0.05	III
Illinois Ave	Adams St	Jefferson St	Pedestrian	0.07	III
Jo Ellen Cir	Genevieve Dr	Genevieve Dr	Pedestrian	0.02	III
Michigan Ave	Jefferson St	Jackson St	Bicycle	0.26	III
Mill Rd	Commerce St	Magnolia St	Pedestrian	0.28	III
Park Blvd	17th St	18th St	Pedestrian	0.12	III
Park Blvd	Eastward Ave	Park Blvd	Pedestrian	0.37	III
Saint Charles St	Highway 49	Depew Rd	Pedestrian	0.35	III
Searle Ave	Glover St	47th St	Pedestrian	0.59	III
Terrace Dr	15th St	Park Blvd	Pedestrian	0.21	III
Washington Ave	54th St	375 Ft Northwest Of Vista Dr	Trail	0.13	III
Washington Ave	54th St	375 Ft Northwest Of Vista Dr	Trail	0.13	III

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Westward Dr	15th St	18th St	Pedestrian	0.24	III
Seaway Rd	Lorraine Rd	Seaway Rd	Bicycle	1.25	III
Rippy Rd	Cuandet Rd	Three Rivers Rd	Trail	1.34	III
Lorraine Rd	I-10	Magnolia St	Bicycle	2.75	III
Trail Along Brickyard Bayou	24th Ave	18th Ave	Trail	0.48	III
17th St	Mills Ave	66th Ave	Bicycle	0.71	III
20th St	30th Ave	42nd Ave	Bicycle	0.82	III
S Swan Rd	Highway 49	N. Swan Rd	Pedestrian	1.62	III
S Swan Rd	Highway 49	N. Swan Rd	Bicycle	1.62	III
Oneal Rd	Old Highway 49	Highway 605	Trail	4.51	III
Seaway Rd	Logan Cline Rd	Three Rivers Rd	Bicycle	2.04	III
Kansas City Southern Railroad	Highway 53	Robinson Rd	Trail	2.52	III
Tegarden Greenway	Tegarden Rd	Great Southern Golf Club Railroad Crossing	Trail	1.54	III
Lorraine Rd	Bayou Bernard Bridge	I-10	Trail	2.57	III
20th St	42nd Ave	49th Ave	Bicycle	0.48	III
40th St	Courthouse Rd	Washington Ave	Bicycle	0.49	III
43rd St	Washington Ave	Searle Ave	Pedestrian	0.42	III
45th St	Washington Ave	Searle Ave	Pedestrian	0.42	III
7th Ave	27th St	28th St	Pedestrian	0.11	III
7th Ave	27th St	28th St	Bicycle	0.11	III
Airport Rd	Cuandet Rd	Washington Ave	Trail	0.27	III
Airport Rd	Customs Blvd	Cuandet Rd	Trail	0.43	III
Airport Rd	Werby Way	Customs Blvd	Trail	0.13	III
H Ave	Maples Dr	Pass Rd	Pedestrian	0.10	III
H Ave	Maples Dr	Pass Rd	Bicycle	0.10	III
Jones St	Anniston Ave	Sarazen Dr	Bicycle	0.23	III
Lorraine Rd	I-10	Dedeaux Rd	Trail	0.52	III
Lorraine Rd	I-10	E. Dedeaux Rd	Trail	0.52	III
Old Hwy 49	S. Swan Rd	Highway 53	Trail	0.55	III
Township Rd	Tegarden Rd	Cowan Rd	Bicycle	0.51	III

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
W Railroad St	Betts St	E. 1st St	Pedestrian	0.75	III
Washington Ave	S. Vista Dr	Cuandet Rd	Trail	0.73	III
Flat Branch Creek	County Barn Rd	N. Swan Rd	Trail	5.70	III
Three Rivers Rd	John Rd	Hwy 605	Pedestrian	0.96	III
Runnymede Dr	Debuys Rd	Switzer Rd	Bicycle	0.69	III
Texas Ave	E. Beach Blvd	33rd St	Bicycle	0.85	III
22nd Ave	28th St	26th St	Bicycle	0.09	III
29th St	B Ave	Hewes Ave	Bicycle	0.10	III
40th St	Commerce St	Commerce St	Pedestrian	0.12	III
40th St	Commerce St	Washington Ave	Pedestrian	0.29	III
48th St	Courthouse Rd	Chamberlain Ave	Pedestrian	0.73	III
48th St	Courthouse Rd	Chamberlain Ave	Bicycle	0.73	III
Chamberlain Ave	Glover St	Washington Ave	Pedestrian	1.06	III
Chamberlain Ave	Glover St	Washington Ave	Bicycle	1.06	III
David St	Klein Rd	Community Rd	Pedestrian	0.52	III
David St	Klein Rd	Community Rd	Bicycle	0.52	III
Harrison Dr	34th Ave	Wisteria St	Bicycle	0.60	III
Kahler St	Courthouse Rd	Mill Rd	Pedestrian	0.29	III
Kahler St	Courthouse Rd	Mill Rd	Bicycle	0.29	III
Parkwood Blvd	Highway 49	Sagewood St	Pedestrian	1.14	III
Perry St	Independence Dr	Cheshire Dr	Pedestrian	0.03	III
Perry St	Independence Dr	Cheshire Dr	Bicycle	0.03	III
Washington Ave	48th St	54th St	Pedestrian	0.48	III
Washington Ave	Hewes Ave	54th St	Bicycle	0.48	III
White St	Cheshire Dr	Tegarden Rd	Pedestrian	0.14	III
White St	Cheshire Dr	Tegarden Rd	Bicycle	0.14	III
29th St to B Ave Greenway	B Ave	Magnolia Dr	Trail	0.22	III
2nd St	Oak Ave	20th Ave	Pedestrian	1.93	III
2nd St	Oak Ave	20th Ave	Bicycle	1.93	III
Old Hwy 49	O'Neal Rd	S. Swan Rd	Trail	1.74	III
Factory Shops Blvd	Creosote Rd	Creosote Rd	Trail	0.89	III

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Lyman Elem. School Connector	Lyman Elementary School	S. Swan Rd	Bicycle	0.10	III
29th St	Hancock Ave	F Ave	Pedestrian	0.31	III
29th St	Hancock Ave	F Ave	Bicycle	0.31	III
30th St	G Ave	Harrison County Child Development Center	Pedestrian	0.06	III
31st St	Courthouse Rd	Hancock Ave	Pedestrian	0.31	III
Admiral Dr	High School Dr	Courthouse Rd	Pedestrian	0.08	III
Anandale Cir	Anandale Cir	Anandale Cir	Pedestrian	0.37	III
Church St	5th St	Old Highway 49	Pedestrian	0.31	III
Cypress Ave	22nd St	7th Ave	Pedestrian	0.36	III
Cypress Ave	22nd St	7th Ave	Bicycle	0.36	III
Fournier Ave	Finley St	W. Railroad St	Pedestrian	0.24	III
Mill Rd	Magnolia St, W. Pine St	Kahler St	Pedestrian	0.52	III
Mill Rd	Magnolia St, W. Pine St	Kahler St	Bicycle	0.52	III
Oak Ave	E. Railroad St	28th St	Pedestrian	0.30	III
Palmer Dr	Ford St	Debuys Rd	Bicycle	0.79	III
S Wilson Blvd	Dedeaux Rd	Community Rd	Pedestrian	0.53	III
S Wilson Blvd	Dedeaux Rd	Community Rd	Bicycle	0.53	III
W Magnolia St	Courthouse Rd	Mill Rd	Pedestrian	0.47	III
Klein Rd	Wilson Blvd	Three Rivers Rd, Prudie Cir	Pedestrian	1.01	III
Klein Rd	Wilson Blvd	Three Rivers Rd, Prudie Cir	Bicycle	1.01	III
State Rte 605 N	John Ross Rd	E. Dedeaux Rd	Trail	1.89	III
State Rte 605 S	Three Rivers Rd	John Ross Rd	Trail	2.63	III
State Rte 605 S	John Ross Rd	Dedeaux Rd	Trail	0.52	III
Airport Rd	Cuandet Rd	No Name	Trail	0.20	III
Airport Rd	0.85 mi W of Washington	0.45 mi W of Washington	Trail	0.37	III
Carl Leggett Rd	Lorraine Rd	Sundown Cir	Pedestrian	0.63	III
State Rte 605 N	0.3 mi N of Oneal Rd	Biloxi River	Trail	0.58	III
Factory Shops Greenway	1.3 mi W of Canal Rd	Factory Shops Blvd	Trail	2.93	III
16th St	Melissa St	J.F.M. Pkwy	Trail	0.07	III
29th St	F Ave	Magnolia Dr	Pedestrian	0.05	III

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
29th St	F Ave	Magnolia Dr	Bicycle	0.05	III
30th St	Courthouse Rd	Hancock Ave, 29th St	Bicycle	0.31	III
45th St	Kendall Ave, Kendall Cir	Washington Ave	Pedestrian	0.39	III
50th St	Kendall Ave	Jefferson Ave, Lawson Ave	Bicycle	0.15	III
50th St	Quincy Ave	Kendall Ave	Bicycle	0.19	III
51st St	Lawson Ave	Washington Ave	Pedestrian	0.34	III
51st St	Lawson Ave	Washington Ave	Bicycle	0.34	III
51st St	Quincy Ave	Lawson Ave	Pedestrian	0.34	III
52nd St	E. 52nd St, Quincy Ave	Washington Ave	Pedestrian	0.68	III
54th St	Lawson Ave	Washington Ave	Bicycle	0.36	III
5th Ave	Old Highway 49	Church St	Pedestrian	0.07	III
5th St	2nd Ave, East Side Ave	Old Highway 49	Pedestrian	0.07	III
Angela Dr	Klein Rd, No Name	Three Rivers Rd	Pedestrian	0.33	III
Angela Dr	Klein Rd, No Name	Three Rivers Rd	Bicycle	0.33	III
Ashbury Ln	Victoria Cir	Carl Legett Rd	Pedestrian	0.27	III
Chestnut Ln	Angela Dr, Three Rivers Rd, Cal Ln	E. Cora Dr	Pedestrian	0.34	III
Coleman Rd	Landon Rd	North April Dr	Trail	0.41	III
Courthouse Rd	Kahler St, 48th St	Hanging Oak Cir	Pedestrian	0.27	III
Courthouse Rd	Kahler St, 48th St	Hanging Oak Cir	Bicycle	0.27	III
E Side Ave	S. Swan Rd	3rd St	Pedestrian	0.33	III
E Side Ave	S. Swan Rd	3rd St	Bicycle	0.33	III
F Ave	29th St, 28th St	29th St	Pedestrian	0.06	III
Ford St	Broad Ave, Central St	Camp Ave	Pedestrian	0.09	III
G Ave	Oak Ave, 28th St	30th St	Pedestrian	0.16	III
Greenglade Dr	Old Highway 49	Dunbarton Dr	Pedestrian	0.28	III
Greenglade Dr	Old Highway 49	Dunbarton Dr	Bicycle	0.28	III
Gulf Ave	E. Beach Blvd	E. Railroad St, Gulf Ave	Pedestrian	0.20	III
Gulf Ave	E. Beach Blvd	E. Railroad St, Gulf Ave	Bicycle	0.20	III
Hardy Ave	W. Beach Blvd	W. Railroad St, Hardy Ave	Pedestrian	0.32	III
Hayden Dr	Palmer Dr	Demaret Dr	Bicycle	0.12	III

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Heatherwood Dr	Old Highway 49	Archwood Dr	Pedestrian	0.16	III
J.F.M. Pkwy	Landon Rd	16th St	Trail	1.15	III
Kendall Ave	50th St	50th St	Pedestrian	0.06	III
Kendall Ave	50th St	50th St	Bicycle	0.06	III
Kendall Ave	Kendall Cir, 45th St	50th St	Pedestrian	0.62	III
Lawson Ave	Jefferson Ave, 50th St	54th St	Pedestrian	0.54	III
Lawson Ave	Jefferson Ave, 50th St	54th St	Bicycle	0.54	III
Perry St	Courthouse Rd	Independence Dr	Pedestrian	0.35	III
Perry St	Courthouse Rd	Independence Dr	Bicycle	0.35	III
Royal Oak Dr	Three Rivers Rd	East Terminus	Pedestrian	0.32	III
S Swan Rd	Woodrow Wilson Dr, Stevens Dr	Old Highway 49	Bicycle	0.48	III
Sarazen Dr	Collins Blvd	Ford St, Greenview Dr	Bicycle	0.29	III
Unnamed Street	Service Road	J. F. M. Pkwy	Trail	0.40	III
Woodmont Dr	Greenglade Dr	Heatherwood Dr	Pedestrian	0.21	III
16 St Greenway	16th St	I-10 Service Road	Trail	0.67	III
Jfm Pkwy Greenway 2	J.F.M. Pkwy	Daniel Greenway	Trail	0.39	III
Parkwood to Mays Greenway	Parkwood Dr N.	Mays Rd	Trail	0.11	III
Daniel Greenway	0.2 mi E of J.F.M. Pkwy	Daniel Blvd	Trail	1.52	III
Mays Rd	Three Rivers Rd, Windsong Dr	Opal Cv	Pedestrian	0.91	III
Kendall Ave	50th St	Bayou Cir	Pedestrian	0.76	III
Depew Rd	Hamilton St	O'Neal Rd	Pedestrian	0.94	III
John Rd	O'Neal Rd	Three Rivers Rd	Pedestrian	0.98	III
N Wilson Blvd	No Name, Klein Rd	Harris Dr	Pedestrian	1.02	III
N Wilson Blvd	No Name, Klein Rd	Harris Dr	Bicycle	1.02	III
Quincy Ave	Courthouse Rd	Bayou Cir, 54th St	Pedestrian	0.49	III
Quincy Ave	Courthouse Rd	Bayou Cir, 54th St	Bicycle	0.49	III
W Taylor Rd	No Name, Lorraine Rd	Ridge Rd, Bluff Ln	Pedestrian	0.43	III
54th St	Bayou Cir, Quincy Ave	Lawson Ave	Bicycle	0.34	III
Bayou Cir	Audubon Pt	56th St	Pedestrian	0.78	III
Brentwood Blvd WB	Dover Dr	Yorkshire Pkwy	Pedestrian	0.05	III
Brentwood Blvd	Carl Legett Rd	Pembroke Cir	Pedestrian	0.04	III

CITY OF GULFPORT RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Brentwood Blvd EB	Dover Dr	Yorkshire Pkwy	Pedestrian	0.06	III
Brentwood Blvd WB	Pembrook Cir	Dover Dr	Pedestrian	0.11	III
Brentwood Blvd EB	Pembrook Cir	Dover Dr	Pedestrian	0.11	III
Courthouse Rd	Hanging Oak Cir	Quincy Ave	Pedestrian	0.34	III
Courthouse Rd	Hanging Oak Cir	Quincy Ave	Bicycle	0.34	III
Courthouse Rd	Quincy Ave	Bayou Cir	Pedestrian	0.42	III
E Taylor Rd	E. Taylor Rd, Bayou Plantation Ln	W. Taylor Rd, Lorraine Rd	Pedestrian	0.79	III
Fournier Ave	W. Beach Blvd	Fournier Ave, Finley St	Pedestrian	0.12	III
Klein Rd	Klein Rd	Wilson Blvd, Harris Dr	Pedestrian	0.04	III
Klein Rd	Klein Rd	Wilson Blvd, Harris Dr	Bicycle	0.04	III
Lawson Ave	Lawson Ave, 54th St	Bayou Cir, 56th St	Pedestrian	0.27	III
N Wilson Blvd	Harris Dr	Klein Rd, Maurine Cir, Harris Dr, Edington Pl, Barimba Pl, Loveless Dr	Bicycle	0.15	III
Turtle Creek Pkwy	Magnolia Ridge Rd	Terrapin Cv, Magnolia Ridge Rd, Willow Oak Cir	Pedestrian	0.07	III

SPOT IMPROVEMENTS

The following table outlines key spot improvements for the City of Gulfport.

LOCATION	CITY	TYPE	DESCRIPTION
US-49 and Community Rd	Gulfport	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
Pass Rd and Hewes Ave	Gulfport	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
US-49 and Airport Rd	Gulfport	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
Lorraine Rd near Sundown Dr	Gulfport	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
15th St and 23rd Ave	Gulfport	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
15th St and 25th Ave	Gulfport	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
Railroad tracks at 15th St and 30th Ave	Gulfport	Intersection Improvement	Public comments indicate further study of safety for pedestrians and bicyclists is needed at this intersection.
US-90 between Venetian Gardens and Debuys Rd	Gulfport	Mid-block Crossing	Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing near Legacy Towers. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.
US-90 at Centennial Plaza Entrance	Gulfport	Intersection Improvement	Conduct traffic engineering study at this location for enhanced high visibility crossing. Could consider PHB or RRFB enhanced mid-block high visibility crossing 100 ft east of the intersection. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.

LOCATION	CITY	TYPE	DESCRIPTION
US-90 near Kelly Ave	Gulfport	Mid-block Crossing	Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.
US-90 between 41st Ave and 38th Ave	Gulfport	Mid-block Crossing	Could be at one of the streets or Mid-block right to the pier near West Side Park. Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.





CITY OF LONG BEACH

Long Beach is recommended to add sidewalks, trails and bicycle facilities in phases. Phase I is envisioned as near-term or highest priority projects. The highest priority projects are also shown in the darkest colors, with lower priority shown in more faded hues.

Bicycle facilities and sidewalks are some of the first projects recommended for implementation by the City of Long Beach. As shown in Figure 44, Phase I projects include bicycle facilities on North Cleveland Avenue, Old Pass Road, and Railroad Street between Klondyke Road and North Cleveland Avenue. Sidewalks are recommended along Railroad Street from North Girard Avenue to Gardendale Avenue. Two trails are recommended

for Phase I: North Cleveland Avenue to East Railroad Street, then East Railroad Street from North Cleveland Avenue to Lewis Avenue. There are several locations where additional pedestrian crossing opportunities are needed. Mid-block crossings over Highway 90/Beach Boulevard near Lewis Avenue, S Girard Avenue and Arbor Station Drive are all included in Phase I projects for the City of Long Beach.

Later phases in Long Beach include further expansion of the bicycle and pedestrian facilities away from the coast, and improvements to the trail along Highway 90/Beach Boulevard. See additional facilities recommended in Figure 45 and Figure 46.

Figure 44 Long Beach Phase I Recommendations

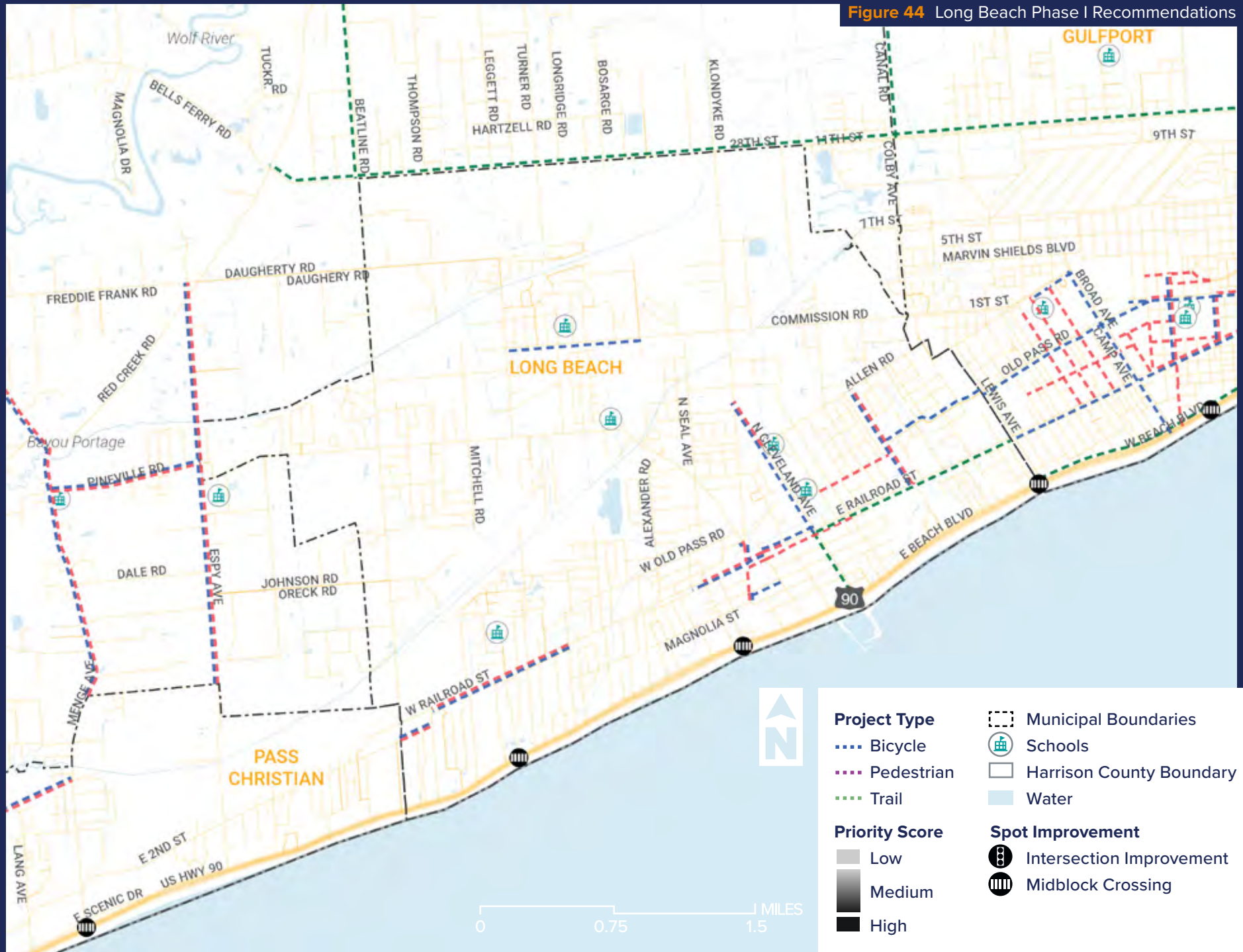


Figure 45 Long Beach Phase II Recommendations

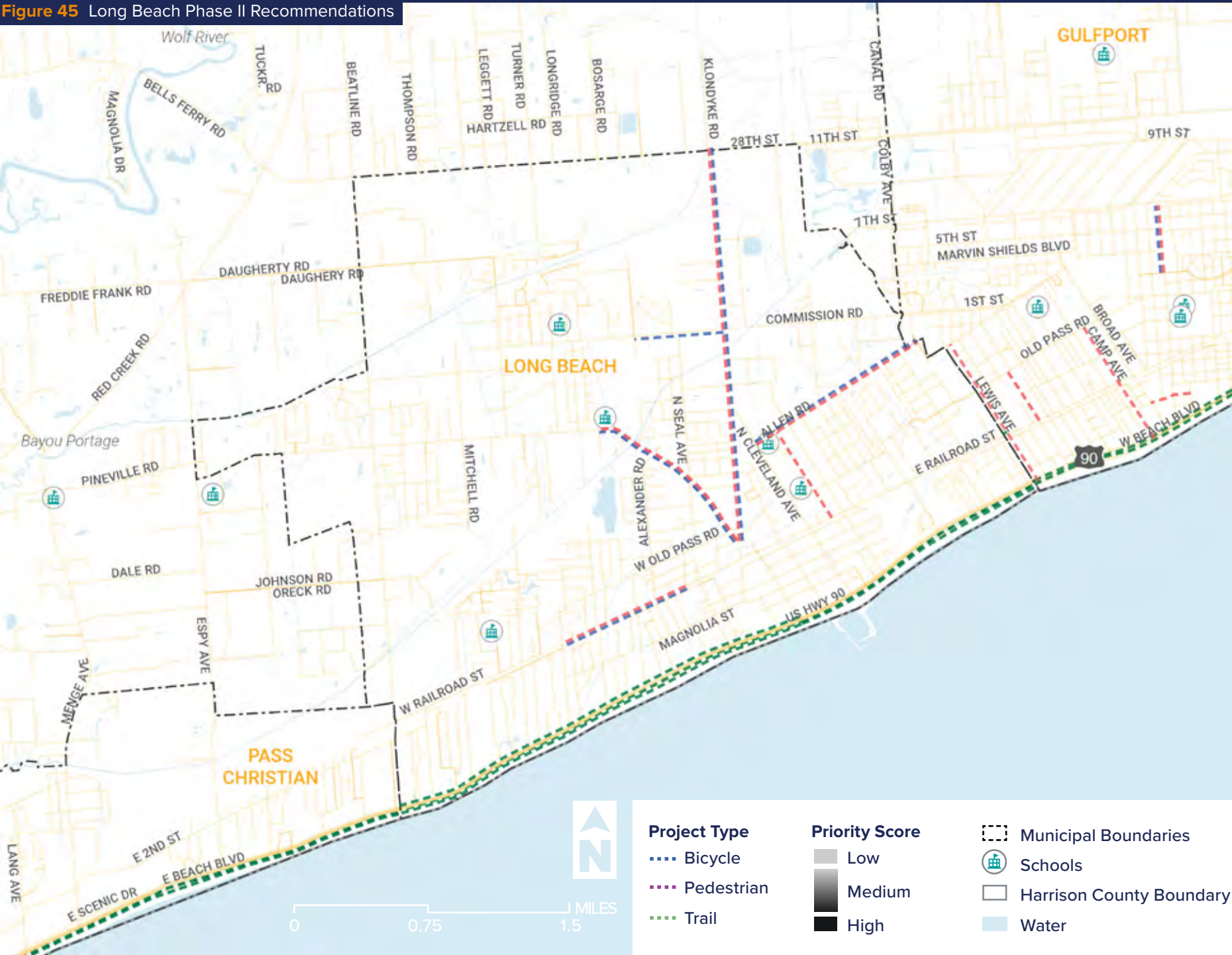
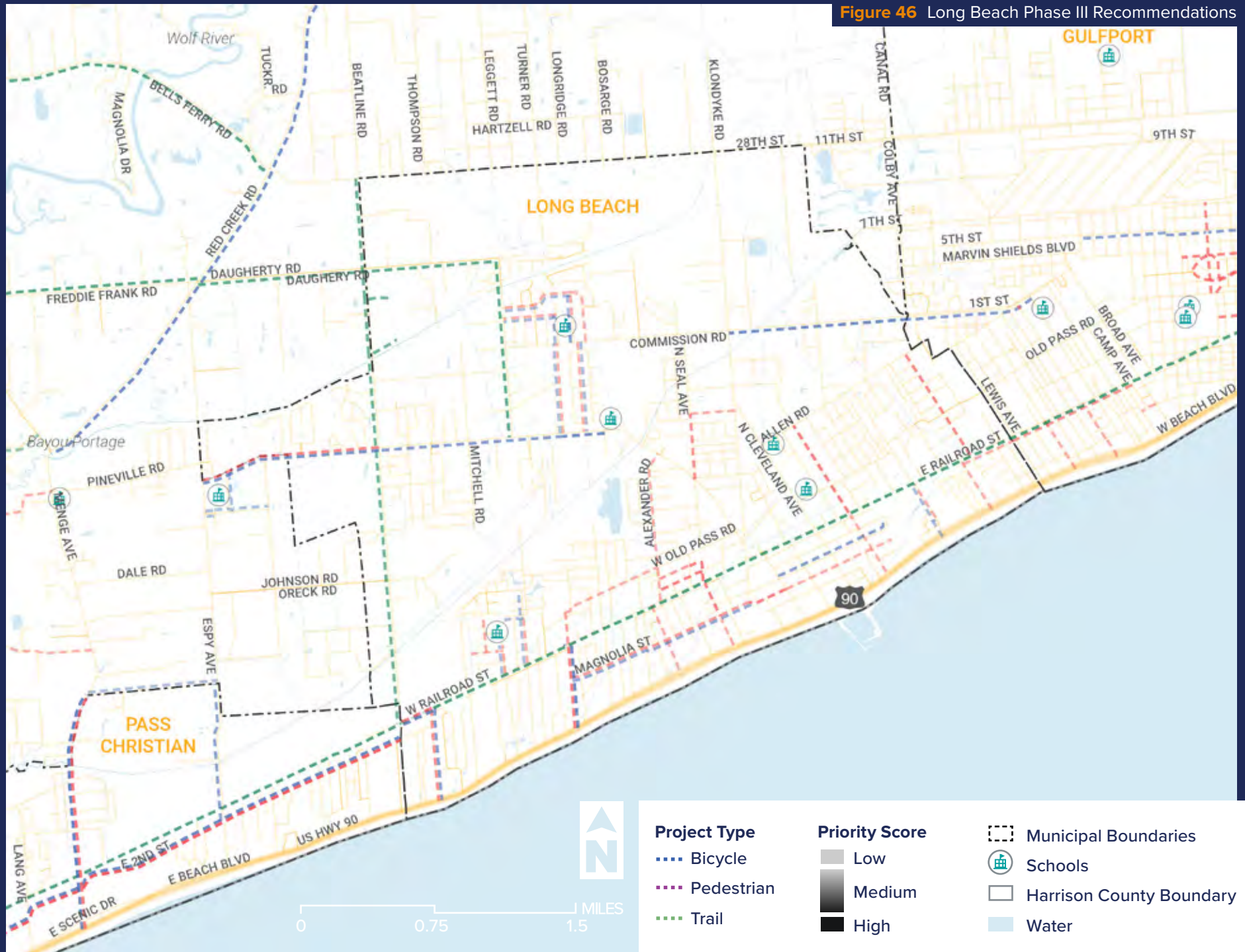


Figure 46 Long Beach Phase III Recommendations



CITY OF LONG BEACH RECOMMENDATIONS

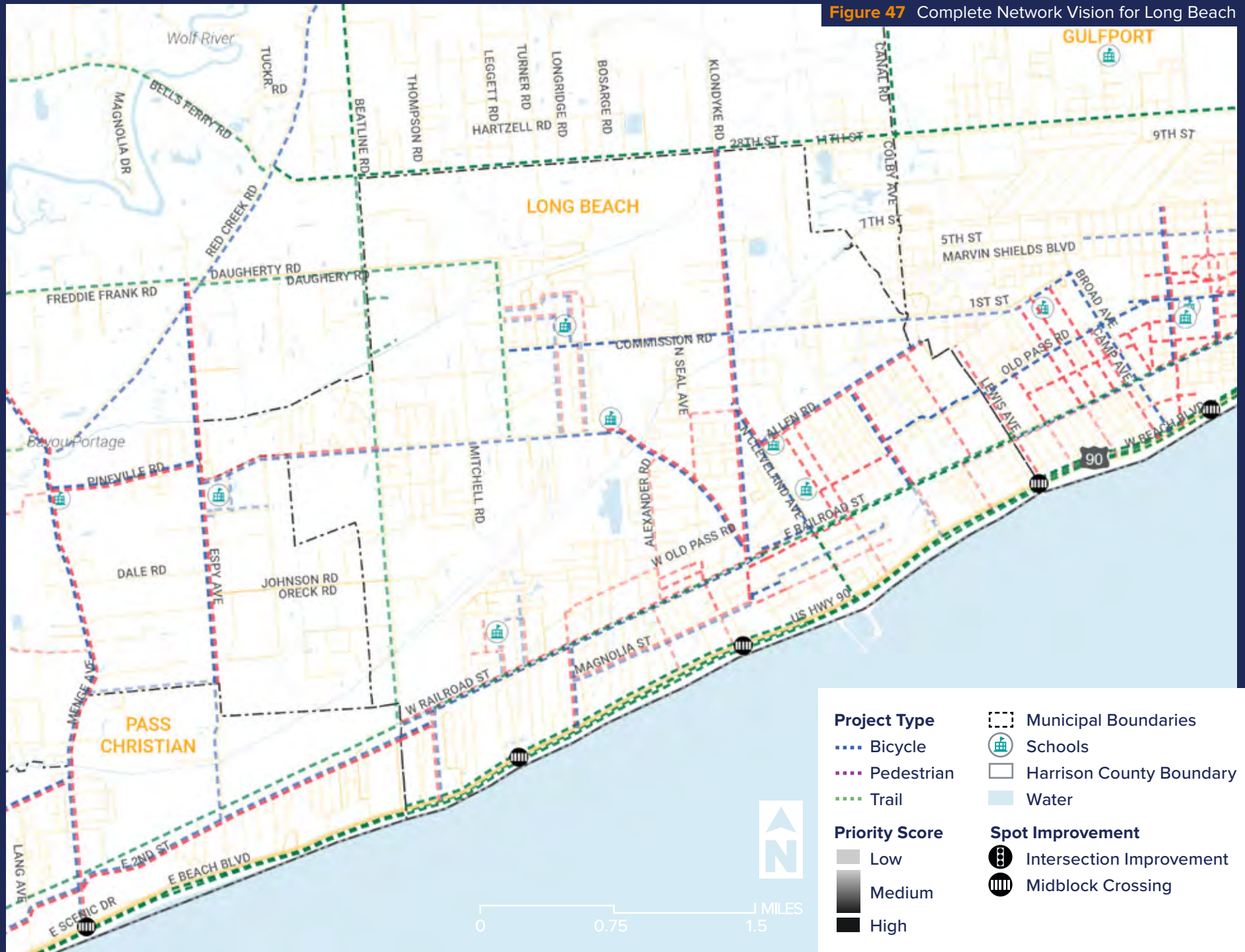
Phase I have prioritization score 0.42 and above

Phase II have prioritization score 0.34 to 0.41

Phase III have prioritization score 0.33 or less

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
E Railroad St	Cleveland Ave	Gardendale Ave	Trail	0.14	I
E Railroad St	Jeff Davis Ave	N. Burke Ave	Pedestrian	0.14	I
E Railroad St	Jeff Davis Ave	N. Burke Ave	Bicycle	0.14	I
N Cleveland Ave	Railroad St	Allen Rd	Bicycle	0.57	I
W Railroad St	Jeff Davis Ave	Girard Ave	Pedestrian	0.38	I
W Railroad St	Jeff Davis Ave	N. Girard Ave	Bicycle	0.38	I
Beach Park Dr	E Railroad St	Railroad Crossing	Pedestrian	0.01	I
Klondyke Rd	Klondyke Rd	W. Railroad St	Pedestrian	0.09	I
Klondyke Rd	Klondyke Rd	W. Railroad St	Bicycle	0.09	I
Pineville Rd	Klondyke Rd	Russell St	Pedestrian	0.02	I
Pineville Rd	Klondyke Rd	Russell St	Bicycle	0.02	I
W Railroad St	N. Girard Ave	4th Ave	Bicycle	0.07	I
E Railroad St	Gardendale Ave	W. Railroad St	Trail	1.07	I
E Old Pass Rd	Gardendale Ave	McCaughan Ave	Pedestrian	0.46	I
E Railroad St	N. Burke Ave	Cleveland Ave	Bicycle	0.18	I
S Cleveland Ave	E. Railroad St	E. Beach Blvd	Trail	0.44	I
W 1st St	Jeff Davis Ave	Church Ave	Pedestrian	0.14	I
15th St	Edmund Cir	Old Pass Rd	Bicycle	0.47	I
E Old Pass Rd	McCaughan Ave	Old Pass Rd	Bicycle	0.62	I
E 1st St	Jeff Davis Ave	S. Nicholson Ave	Pedestrian	0.53	I
Church Ave	E. 1st St	Oak St, W. 3rd St	Pedestrian	0.12	I
Church Ave	Oak St	Magnolia St	Pedestrian	0.05	I
Church Ave	Oak St	Magnolia St	Bicycle	0.05	I
Commission Rd	S. Harvest Ln	Daugherty Rd	Bicycle	0.76	I
McCaughan Ave	E. Railroad St	Allen Rd	Pedestrian	0.63	I

Figure 47 Complete Network Vision for Long Beach



CITY OF LONG BEACH RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Mccaughan Ave	E. Railroad St	Allen Rd	Bicycle	0.63	I
N Cleveland Ave	Allen Rd	Klondyke Rd	Pedestrian	0.29	I
N Cleveland Ave	Allen Rd	Klondyke Rd	Bicycle	0.29	I
W 2nd St	E. 2nd St	White Harbor Rd	Pedestrian	0.20	I
W 2nd St	E. 2nd St	White Harbor Rd	Bicycle	0.20	I
W 3rd St	Jeff Davis Ave	Church Ave	Bicycle	0.19	I
W Railroad St	Lang Ave	White Harbor Rd	Pedestrian	0.85	I
W Railroad St	S. Lang Ave	White Harbor Rd	Bicycle	0.85	I
Pineville Rd	Klondyke Rd	Royal Dr	Pedestrian	1.04	II
Pineville Rd	Klondyke Rd	Royal Dr	Bicycle	1.04	II
Klondyke Rd	28th St	Pineville Rd	Pedestrian	2.23	II
Klondyke Rd	28th St	Pineville Rd	Bicycle	2.23	II
W Beach Blvd	Holiday Dr	Jeff Davis Ave	Trail	2.50	II
W Beach Blvd	Holiday Dr	Jeff Davis Ave	Trail	2.50	II
Allen Rd	N. Cleveland Ave	65th Ave	Pedestrian	1.07	II
Allen Rd	N. Cleveland Ave	65th Ave	Bicycle	1.07	II
Commission Rd	Klondyke Rd	Harvest Ln	Bicycle	0.50	II
E Beach Blvd	Davis Ave	Lewis Ave	Trail	1.53	II
E Beach Blvd	Jeff Davis Ave	Lewis Ave	Trail	1.53	II
Gardendale Ave	E. Railroad St	Allen Rd	Pedestrian	0.57	II
W Railroad St	4th Ave	Lang Ave	Pedestrian	0.77	II
W Railroad St	4th Ave	S. Lang Ave	Bicycle	0.77	II
Commission Rd	66th Ave	Klondyke Rd	Bicycle	1.01	III
Girard Ave	W. Railroad St	Magnolia St	Pedestrian	0.22	III
Kohler St	N. Girard Ave	Alexander Rd	Pedestrian	0.27	III
N Girard Ave	W. Railroad St	4th Ave	Pedestrian	0.18	III
N Lang Ave	W. Railroad St	W. Railroad St	Pedestrian	0.01	III
N Lang Ave	W. Railroad St	W. Railroad St	Bicycle	0.01	III
N Nicholson Ave	E. Railroad St	Allen Rd	Pedestrian	0.58	III
Pineville Rd	Willow Creek Dr	Clower Ave	Bicycle	0.23	III
S Lang Ave	W. Railroad St	W. Beach Blvd	Pedestrian	0.47	III

CITY OF LONG BEACH RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
S Lang Ave	W. Railroad St	W. Beach Blvd	Bicycle	0.47	III
W 4th St	Jeff Davis Ave	Church Ave	Pedestrian	0.23	III
W Railroad St	White Harbor Rd	Beatline Rd	Pedestrian	0.19	III
W Railroad St	White Harbor Rd	Beatline Rd	Bicycle	0.19	III
CSXT L And N Railroad	30th Ave	Market St	Trail	9.63	III
Pineville Rd	Beatline Rd	Espy Ave	Pedestrian	1.02	III
Pineville Rd	Beatline Rd	Espy Ave	Bicycle	1.02	III
N Wright Ave	E. Railroad St	Allen Rd	Pedestrian	0.68	III
Daugherty Rd	Red Creek Rd	Pineville Rd	Trail	2.71	III
E 3rd St	Jeff Davis Ave	E. 2nd St	Bicycle	0.72	III
N Seal Ave	Todd Ter	Pineville Rd, Larosa Rd	Pedestrian	0.37	III
Pineville Rd	Clower Ave	Beatline Rd	Bicycle	1.04	III
Quarles St	Commission Rd	Alverado Dr	Bicycle	0.15	III
White Harbor Rd	W. Railroad St	W. Beach Blvd	Pedestrian	0.53	III
White Harbor Rd	W. Railroad St	W. Beach Blvd	Bicycle	0.53	III
Beatline Rd	28th St	W. Railroad St	Trail	3.37	III
Magnolia St	Church Ave, W. 4th St	S. Lang Ave	Pedestrian	1.10	III
Magnolia St	Church Ave, W. 4th St	S. Lang Ave	Bicycle	1.10	III
N Lang Ave	W. Railroad St, S. Lang Ave	Church St	Pedestrian	0.18	III
Russell St	Alexander Rd, Woodward Cir	4th Ave, N. Girard Ave, Russell St	Pedestrian	0.19	III
Reeves Elementary Access Trail	N. Seashore Ave, E. Marigold Dr	Barbara Ct, Twin Cedar Ave	Bicycle	0.11	III
Alexander Rd	W. Railroad St	Pineville Rd	Pedestrian	0.85	III
Alverado Dr	Commission Rd	Quarles St	Pedestrian	0.32	III
Alverado Dr	Quarles St	Daugherty Rd	Pedestrian	0.41	III
Alverado Dr	Quarles St	Daugherty Rd	Bicycle	0.41	III
Alyce Pl	St. Augustine Dr	Shady Ln	Pedestrian	0.18	III
Beach Park Pl	Avenue Of The Oaks	Southern Way, Avenue Of The Oaks	Bicycle	0.04	III
Beach Park Pl	Railroad Crossing	Beach Blvd	Bicycle	0.34	III
Beach Park Pl	Southern Way, Avenue Of The Oaks	McCaughan Ave, E. Railroad St	Pedestrian	0.22	III
Clower Ave	Pineville Rd	Commission Rd	Pedestrian	0.50	III
Clower Ave	Pineville Rd	Commission Rd	Bicycle	0.50	III

CITY OF LONG BEACH RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
E 1st St	Richards Ave	W. Railroad St	Pedestrian	0.30	III
E 4th St	Jeff Davis Ave, W. 4th St	E. 5th St, S. Nicholson Ave, E. 3rd St	Pedestrian	0.65	III
N Seashore Ave	W. Railroad St	W. Marigold Dr, E. Marigold Dr	Pedestrian	0.27	III
N Seashore Ave	W. Railroad St	W. Marigold Dr, E. Marigold Dr	Bicycle	0.27	III
N Seashore Blvd	W. Merigold Dr, E. Merigold Dr	Majorie St, Twin Lakes Blvd	Bicycle	0.14	III
Reeves St	Commission Rd	Saratoga Dr	Bicycle	0.16	III
Royal Dr	Commission Rd	Pineville Rd	Pedestrian	0.51	III
Royal Dr	Commission Rd	Pineville Rd	Bicycle	0.51	III
S Nicholson Ave	E. Railroad St, S. Nicholson Ave	No Name, E. Beach Blvd	Pedestrian	0.42	III
Saratoga Dr	Reeves St	Daugherty Rd	Pedestrian	0.30	III
Saratoga Dr	Reeves St	Daugherty Rd	Bicycle	0.30	III
St Augustine Dr	Alyce Pl	Barbara Ct	Pedestrian	0.21	III
St Augustine Dr	W. Railroad St	Alyce Pl	Pedestrian	0.16	III
St Augustine Dr	W. Railroad St	Alyce Pl	Bicycle	0.16	III
Todd Ter	N Seal Ave	Klondyke Rd	Pedestrian	0.25	III
W Old Pass Rd	N Lang Ave	Alexander Rd	Pedestrian	0.59	III
Richards Ave	No Name, E. Beach Blvd	Richards Ave, E. Railroad St.	Pedestrian	0.33	III
S Girard Ave	S. Girard Ave, Magnolia St	W. Beach Blvd, No Name	Pedestrian	0.20	III
Trautman Ave	W. Railroad St, Tauman Ave	No Name, W. Beach Blvd	Pedestrian	0.43	III

SPOT IMPROVEMENTS

The following table outlines key spot improvements for the City of Long Beach.

LOCATION	CITY	TYPE	DESCRIPTION
Central Ave or Lewis Ave	Long Beach	Mid-block Crossing	<p>Central Avenue has sidewalks, but Lewis Avenue connects all the way up to the William Bell Apartments. Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.</p> <p>*Note: Lewis Avenue is within the City of Gulfport's limits.</p>
US-90 between S. Girard Ave and Russel Ave	Long Beach	Mid-block Crossing	<p>Connect The Inn at Long Beach with the beach. Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.</p>
US-90 near Arbor Station Dr	Long Beach	Mid-block Crossing	<p>Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.</p>



CITY OF PASS CHRISTIAN

The Initiative recommends a total of 75 projects for Pass Christian. The projects are divided into different phases, with Phase I envisioned as near-term or highest priority projects. The highest priority projects are shown in the darkest colors, with lower priority shown in more faded hues. Phase I projects are shown in Figure 48.

North Street is a key corridor for both pedestrian and bicycle improvements in Pass Christian: both new sidewalks and bicycle facilities, either bicycle lanes or sidepaths, are recommended along Cedar Avenue from Everett Street to North Street, then along North Street from Cedar Avenue to Menge Avenue. Bicycle facilities are also recommended along Davis Avenue, East 2nd Street and Bayview Street. Another high-priority project is a trail, either a sidepath or greenway, that would follow Henderson Avenue from north of Pass Christian to the US-90 / Beach Boulevard. In addition, updates to the

trail along US-90 / Beach Boulevard are recommended from the Bay St. Louis-Pass Christian bridge to the Clarke Avenue area. There are three key mid-block crossings recommended for Pass Christian in Phase I all along US-90 / Beach Boulevard near at Avenue, Davis Avenue and Menge Avenue.

Phase II projects make additional connections for pedestrians and bicyclists along Royal Oak Boulevard and East 2nd Street, as shown in Figure 49. Improvements to the multi-use path / trail along US-90 / Beach Boulevard from the Everett Street area to Espy Avenue area are also recommended. Phase III projects primarily include additional sidewalk connections and bicycle facilities along local roads, including Everett Street, Fairway Drive, Clarence Avenue, Market Street, Fleitas Avenue, East 2nd Street and Menge Avenue. These projects are shown in Figure 50.

Figure 48 Pass Christian Phase I Recommendations

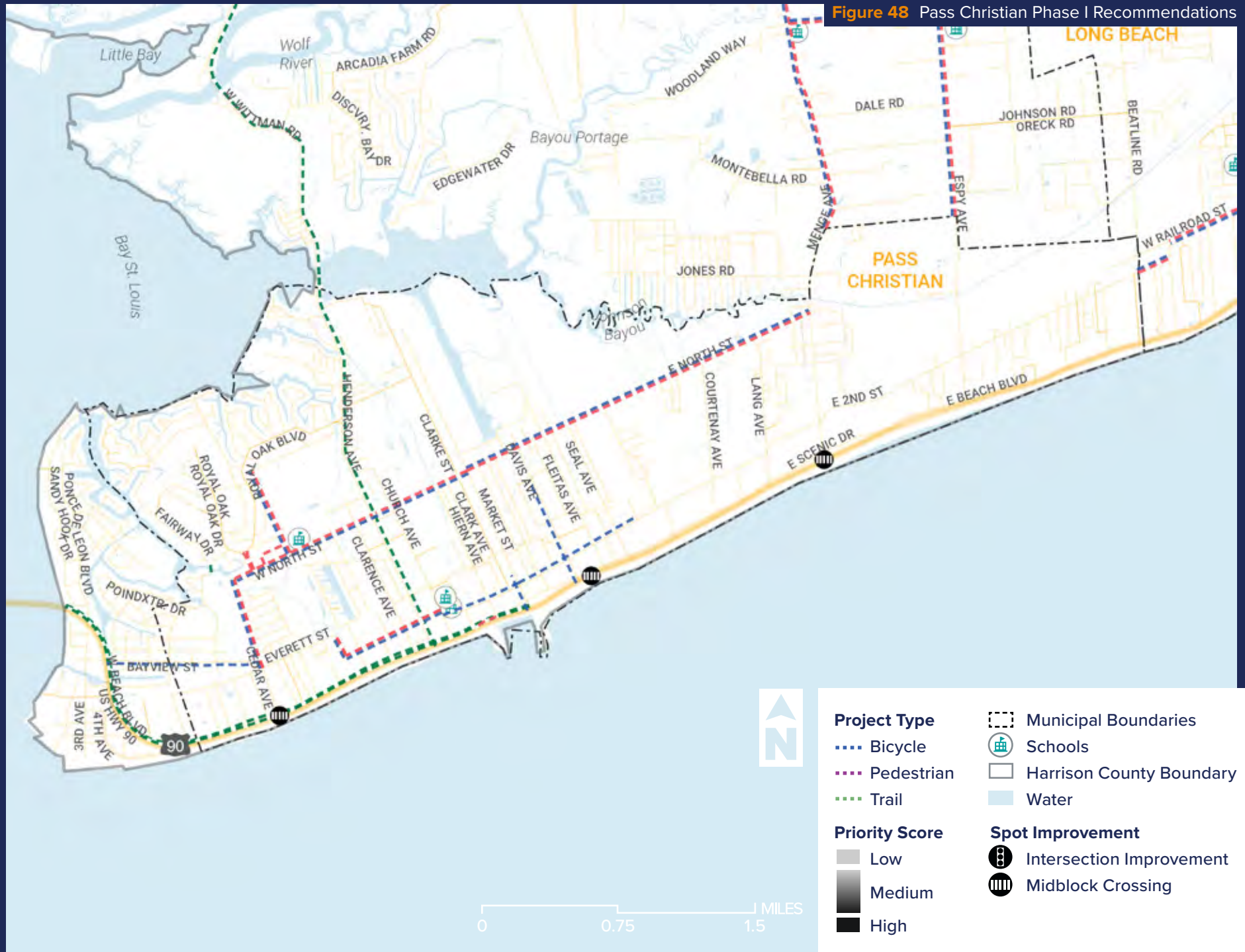
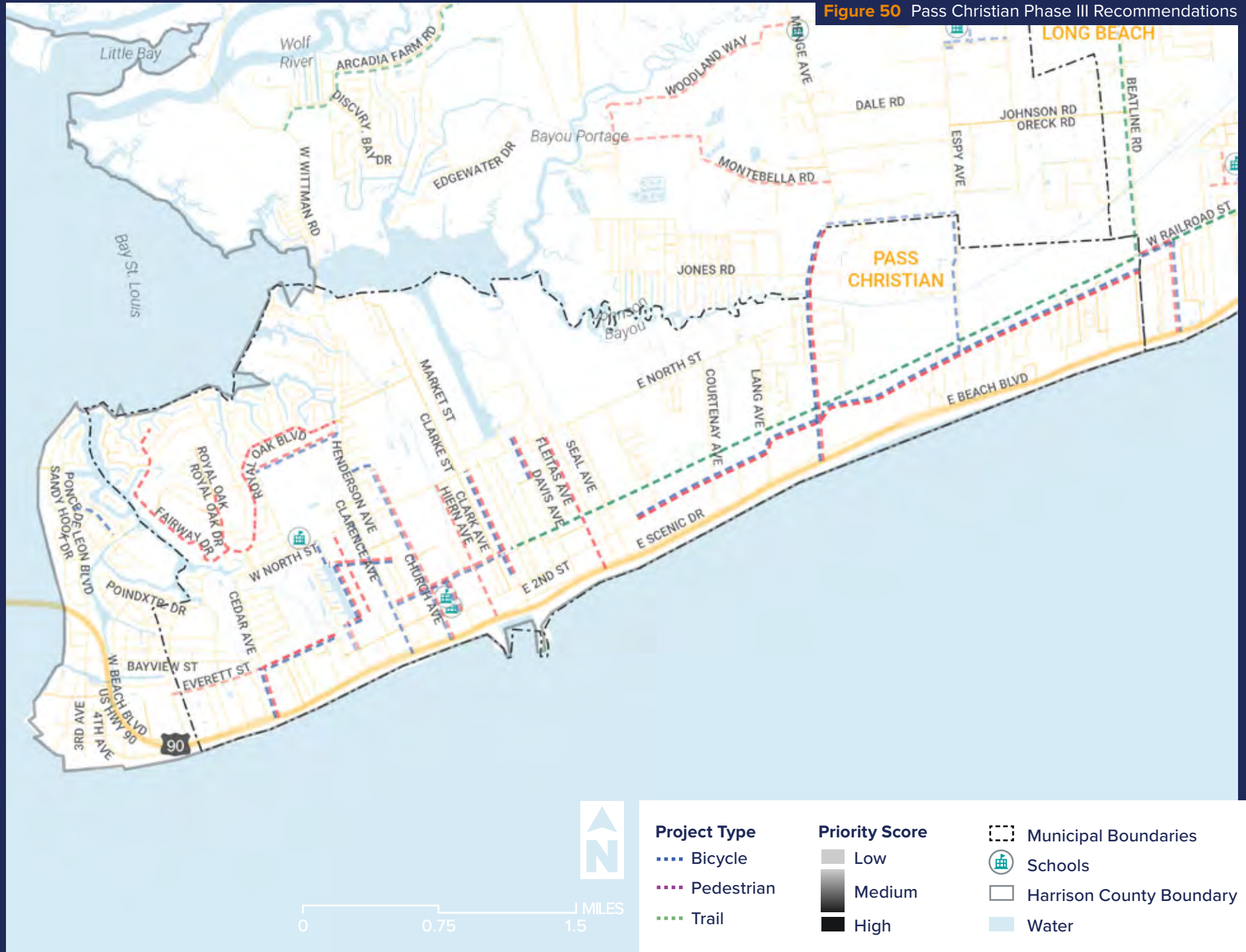


Figure 49 Pass Christian Phase II Recommendations



Figure 50 Pass Christian Phase III Recommendations



CITY OF PASS CHRISTIAN RECOMMENDATIONS

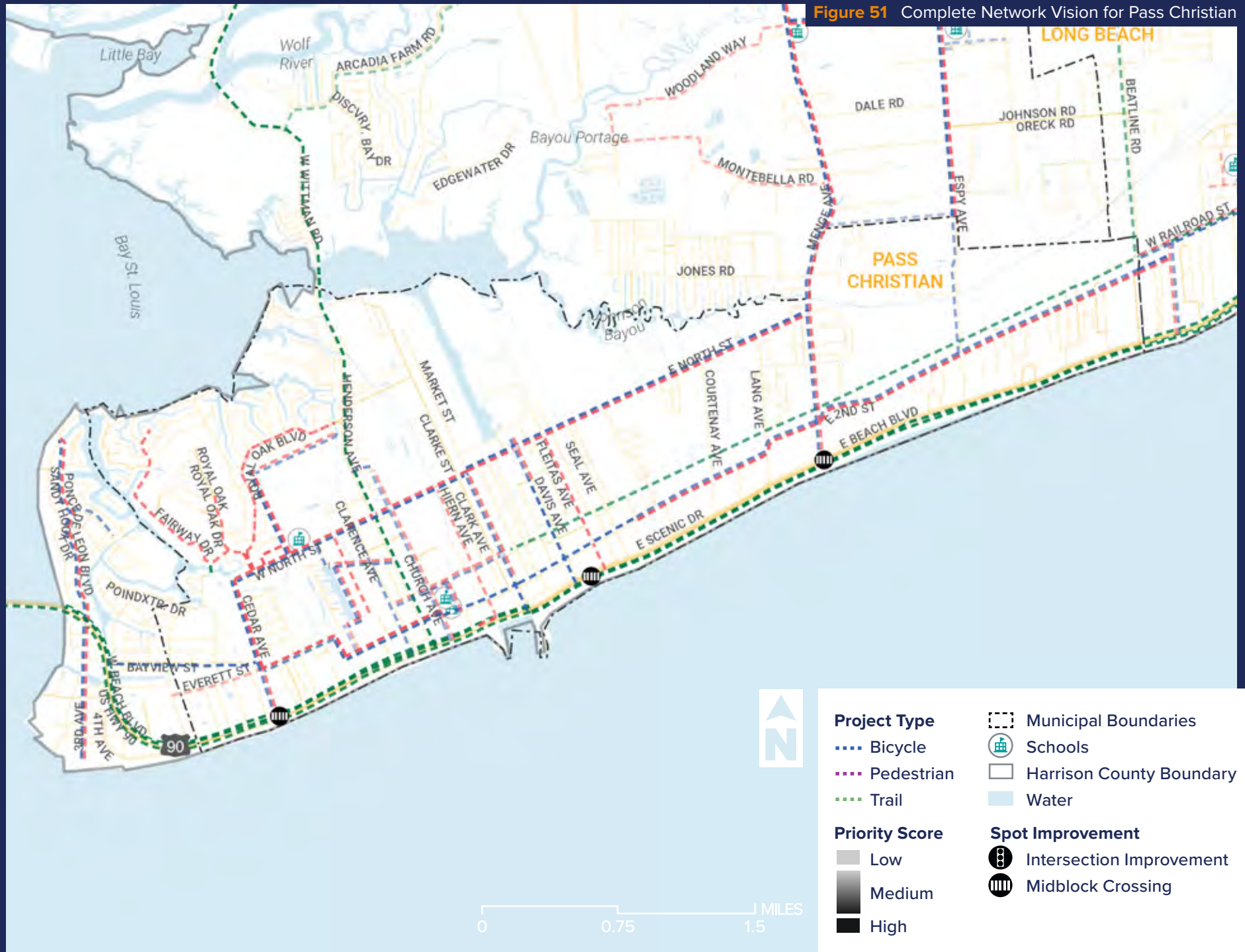
Phase I have prioritization score 0.42 and above

Phase II have prioritization score 0.34 to 0.41

Phase III have prioritization score 0.33 or less

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
E 2nd St	Market St	Algernon Ave	Bicycle	0.76	I
W 2nd St	Church Ave	Market St	Bicycle	0.46	I
W Scenic Dr	Hiern Ave	Beach Blvd	Pedestrian	0.14	I
W North St	Market St	Cedar Ave	Pedestrian	1.45	I
W North St	Market St	Cedar Ave	Bicycle	1.45	I
Davis Ave	E. Beach Blvd	Rafferty Dr	Bicycle	0.64	I
Cedar Ave	Everett St	W. North St	Pedestrian	0.52	I
Cedar Ave	Everett St	W. North St	Pedestrian	0.52	I
Cedar Ave	Everett St	W. North St	Bicycle	0.52	I
Bayview St	Cedar Ave	W. Beach Blvd	Bicycle	0.86	I
Henderson Ave	W. Beach Blvd	Henderson Ave Bridge	Trail	2.23	I
E North St	Market St	Menge Ave	Pedestrian	2.13	I
E North St	Market St	Menge Ave	Bicycle	2.13	I
W Beach Blvd	Hancock County	Market St	Trail	3.26	I
Market St	Beach Blvd	400 Ft North of E 2nd St	Bicycle	0.22	I
Northwood St	W. North St	Oakland Ave	Pedestrian	0.21	I
Oakland Ave	W. North St	Wood St	Pedestrian	0.50	I
Oakland Ave	W. North St	Wood St	Bicycle	0.50	I
Saint Louis Ave	Church Ave	W End Rd	Pedestrian	0.59	I
Saint Louis Ave	Church Ave	West End Dr	Pedestrian	0.59	I
Saint Louis Ave	Church Ave	West End Dr	Bicycle	0.59	I
Timber Ridge Blvd	W. North St	Royal Oak Blvd	Pedestrian	0.12	I
Timber Ridge Blvd	W. North St	Royal Oak Blvd	Pedestrian	0.12	I
W Beach Blvd	Market St	Boisdore Ave	Trail	2.02	I

Figure 51 Complete Network Vision for Pass Christian



CITY OF PASS CHRISTIAN RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
W End Dr	St Louis Ave	Everett St	Pedestrian	0.13	I
W End Dr	St. Louis Av	Everett St	Bicycle	0.13	I
W End Dr	St. Louis St	Everett St	Pedestrian	0.13	I
Golf Cart Path	Hillcrest Dr	Fairway Dr	Trail	0.07	I
E Beach Blvd	Market St	Holiday Dr	Trail	3.79	II
E Beach Blvd	Holiday Dr	Market St	Trail	3.78	II
Menge Ave	E. 2nd St	Demourelle Rd	Pedestrian	1.11	III
Menge Ave	E. 2nd St	Demourelle Rd	Bicycle	1.11	III
Fairway Dr	Royal Oak Blvd	Ashwood Dr	Pedestrian	1.14	III
Fleitas Ave	E. Beach Blvd	E. North St	Pedestrian	0.83	III
E 2nd St	Algernon Ave	W. 2nd St	Pedestrian	3.19	III
E 2nd St	Algernon Ave	W. 2nd St	Pedestrian	3.19	III
E 2nd St	Algernon Ave	W. 2nd St	Bicycle	3.19	III
Cedar Ave	W. Beach Blvd	Everett St	Pedestrian	0.30	III
Cedar Ave	W. Beach Blvd	Everett St	Bicycle	0.30	III
Davis Ave	North St	Saucier Ave	Bicycle	0.27	III
Davis Ave	Rafferty Dr	North St	Pedestrian	0.27	III
Earl Cir	Henderson Ave	Church Ave	Bicycle	0.12	III
Everett St	West End Dr	Cedar Ave	Pedestrian	0.44	III
Everett St	West End Dr	Cedar Ave	Pedestrian	0.44	III
Everett St	West End Dr	Cedar Ave	Bicycle	0.44	III
Josephine St	Henderson Ave	Pirate Cv	Pedestrian	0.36	III
Josephine St	Henderson Ave	Pirate Cv	Bicycle	0.36	III
Lac Bleu Ave	W. Railroad St	Josephine St	Pedestrian	0.34	III
Lac Bleu Ave	W. Railroad St	Josephine St	Bicycle	0.34	III
Market St	Clark Ave	North St	Pedestrian	0.64	III
Market St	Clark Ave	North St	Bicycle	0.64	III
Menge Ave	E. Beach Blvd	E. 2nd St	Pedestrian	0.22	III
Menge Ave	E. Beach Blvd	E. 2nd St	Bicycle	0.22	III
Pine Ave	W. Railroad St	W. North St	Pedestrian	0.49	III
Pirate Cv	Josephine St	W. North St	Bicycle	0.15	III

CITY OF PASS CHRISTIAN RECOMMENDATIONS CONTINUED

STREET/RIGHT-OF-WAY	FROM	TO	PROJECT TYPE	LENGTH (MILES)	PHASE
Royal Oak	Redbud Way	Henderson Ave	Pedestrian	1.56	III
W Railroad St	Lac Bleau Ave	Magnolia Ave	Pedestrian	0.09	III
W Railroad St	Lac Bleau Ave	Magnolia Ave	Bicycle	0.09	III
Royal Oak Blvd	Greenwood Dr	Henderson Ave	Pedestrian	1.03	III
Clarence Ave	W. Beach Blvd	W. North St	Bicycle	0.82	III
Leovy Ave	W. 2nd St	W. Railroad St	Pedestrian	0.19	III
Leovy Ave	W. 2nd St	W. Railroad St	Bicycle	0.19	III
W Railroad St	Market St	Clarence Ave	Pedestrian	0.69	III
W Railroad St	Market St	Clarence Ave	Bicycle	0.69	III
W Railroad St	W. Railroad St	Church Ave	Pedestrian	0.10	III
Wood St	Hackberry Dr	Oakland Ave	Pedestrian	0.51	III
Wood St	Hackberry Dr	Oakland Ave	Bicycle	0.51	III
Espy Ave	E. 2nd St	Demourelle Rd	Bicycle	0.78	III
Hiern Ave	W. Scenic Dr	W. North St	Pedestrian	0.82	III
Church Ave	W. Beach Blvd, No Name	Earl Cir, Church Ave	Pedestrian	1.06	III
Church Ave	W. Beach Blvd, No Name	Earl Cir, Church Ave	Bicycle	1.06	III
Everett St	Cedar Ave	Lady Mary Ave	Pedestrian	0.54	III
Magnolia Ave	St. Louis St	W. Railroad St	Pedestrian	0.16	III
Magnolia Ave	St. Louis St	W. Railroad St	Bicycle	0.16	III
Demourelle Rd	Menge Ave	Espy Ave	Bicycle	0.73	III

SPOT IMPROVEMENTS

The following table outlines key spot improvements for the City of Pass Christian.

LOCATION	CITY	TYPE	DESCRIPTION
US-90 near Menge Ave	Pass Christian	Mid-block Crossing	Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.
US-90 between Davis Ave & Fleitas Ave	Pass Christian	Mid-block Crossing	Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.
US-90 near Cedar Ave	Pass Christian	Mid-block Crossing	Conduct traffic engineering study at this location for PHB or RRFB enhanced mid-block high visibility crossing. Coordinate with upcoming 2022 MDOT US-90 Traffic Signal, ITS & Signage project 108757/302000.



Policy and Program Recommendations

The following policy and program recommendations could be implemented countywide. They are intended to apply in both incorporated and unincorporated portions of Harrison County. These recommendations have been developed using a variety of stakeholder feedback collected between December 2020 and June 2021.

POLICY RECOMMENDATIONS

Policy recommendations focus on the legal ordinances and standards of Harrison County. These recommendations signify to visitors and residents alike that Harrison County and the cities of Biloxi, D'Iberville, Gulfport, Long Beach, and Pass Christian are committed to improving walking and biking long-term (10+ years). The following policies affect different parts of the pedestrian and bicyclist experience, such as conflicts with motor vehicles, connectivity to different areas, ease of bicycle parking, and safety.



TOPIC AREA	POLICY RECOMMENDATION	RESPONSIBLE PARTY	TIMEFRAME	FUNDING NEEDS
LOCAL DEVELOPMENT ORDINANCES	Municipal and county planners and planning commission members can update local ordinances specifically to develop Complete Streets and Greenways.	Lead: Municipal and County Planners Support: Planning Commission members	Short-term (0-5 years)	Low
	Municipal and county planners and planning commission members can update local ordinances specifically to incorporate pedestrian- and bicycle-oriented design elements.	Lead: Municipal and County Planners Support: Planning Commission members	Short-term (0-5 years)	Low
	Municipal and county planners and planning commission members can update local ordinances specifically to increase community street connectivity.	Lead: Municipal and County Planners Support: Planning Commission members	Short-term (0-5 years)	Low
	Municipal and county planners and planning commission members can update local ordinances specifically to specify open spaces, easements, or additional connectivity within new developments.	Lead: Municipal and County Planners Support: Planning Commission members	Short-term (0-5 years)	Low

TOPIC AREA	POLICY RECOMMENDATION	RESPONSIBLE PARTY	TIMEFRAME	FUNDING NEEDS
BICYCLE PARKING	Implement bike rack policy allowing businesses and residents to request bike parking, and allow bike parking to be provided within public right-of-way.	Lead: Gulf Regional Planning Commission Support: Municipal parking enforcement, County and Municipal Community Development, Municipal and County Public Works	Long-term (10+ years)	Medium
	Develop guidelines for retrofitting on-street parking to bike corrals where higher bike volumes are expected.	Lead: Gulf Regional Planning Commission Support: Municipal parking enforcement, County and Municipal Community Development, Municipal and County Public Works	Long-term (10+ years)	Medium
TRAFFIC CALMING AND SPEED REDUCTION	Develop and implement clear and concise guidelines for installation of traffic calming measures.	Lead: County and Municipal Public Works, Planning Support: Gulf Regional Planning Commission, County and Municipal Police Departments	Long-term (10+ years)	Medium

TOPIC AREA	POLICY RECOMMENDATION	RESPONSIBLE PARTY	TIMEFRAME	FUNDING NEEDS
TRAFFIC CALMING AND SPEED REDUCTION	Identify five (5) pilot projects to implement 20 mph zones near intersections with relatively high pedestrian and bicycle rider crash history and/or near activity centers that generate walking and biking activity.	Lead: County and Municipal Public Works, Planning Support: Gulf Regional Planning Commission, County and Municipal Police Departments	Long-term (10+ years)	Medium
PERFORMANCE METRICS	Develop specific performance metrics that will be used for all roadway projects which encourage the inclusion of bicycle and pedestrian infrastructure.	Lead: County and Municipal Planning Departments Support: Gulf Regional Planning Commission, County and Municipal Public Works, Mississippi DOT	Short-term (0-5 years)	Low
LOCAL ENGINEERING PRACTICES	Update local bicycle and pedestrian design standards. Adopt by resolution the NACTO Urban Bikeway Design Guide, the FHWA Separated Bike Lane Planning and Design Guide, and/or the FHWA Small Town and Rural Multimodal Networks guide as supplemental resources to implement infrastructure.	Lead: County and Municipal Public Works Support: County and Municipal Planning Departments	Short-term (0-5 years)	Low

PROGRAM RECOMMENDATIONS

Program recommendations focus on specific activities that Harrison County and the cities of Biloxi, D'Iberville, Gulfport, Long Beach, and Pass Christian can implement to coordinate transportation planning, encourage use of existing facilities, and help people safely discover how to bike and walk. Programs that are tailored to people of all ages and abilities will help them to realize the

full potential of new and proposed walkways and bikeways. The recommended programmatic actions will increase the visibility of people who walk and bike, communicate that all road users are expected to look for each other no matter how they travel, create safer streets, and develop a common understanding of traffic safety.

TOPIC AREA	PROGRAM RECOMMENDATION	RESPONSIBLE PARTY	TIMEFRAME	FUNDING NEEDS
STAFF/HUMAN RESOURCES FOR BICYCLE AND PEDESTRIAN PLANNING & PROGRAMS	Encourage local jurisdictions to designate (or create a new position for) a bicycle/pedestrian program coordinator, or, at a minimum, dedicate responsibility for bicycle/pedestrian facility planning, implementation, and programming to an existing position. The coordinator would be the driver of coordination across jurisdictions, developing a program specific to the local setting.	Lead: Gulf Regional Planning Commission Support: Municipal and County Public Works, Community Development, Parks & Recreation	Short-term (0-5 years)	Low
	Organize a volunteer advisory committee to secure more grant funding. Develop a specific workplan to provide education and guidance for local planning partners to proactively assist underserved communities with active transportation planning and grant applications.	Lead: County Planning Department Support: Gulf Regional Planning Commission	Mid-term (5-10 years)	Low

TOPIC AREA	PROGRAM RECOMMENDATION	RESPONSIBLE PARTY	TIMEFRAME	FUNDING NEEDS
ENCOURAGEMENT	Host regular walking- and bicycling-focused events, at least 2-3 times per year and increasing in frequency over time. These may include nighttime “lighted bike rides”, charity running/walking events, and bridge walks/rides.	Lead: Active Living Committee Support: Municipal and County Parks & Recreation; Harrison County Department of Public Health; Nonprofit or other Private Partners	Short-term (0-5 years)	Medium
	Plan several Open Streets events along different portions of the Highway 90 corridor, at seasonal times of the year. Open Streets Events temporarily close streets to vehicular traffic, usually for a few hours on a weekend. Open Streets allow for: demonstrations of new infrastructure; interactions between officials and public stakeholders; testing and training of bicycling skills; and more.	Lead: Active Living Committee Support: Municipal and County Parks & Recreation; Harrison County Department of Public Health; Nonprofit or other Private Partners	Mid-term (5-10 years)	Low
	Develop an Earn-a-Bike program. Working with nonprofit or for-profit bike shops, local governments could fund a program focused on teaching children and/or adults the basics of bicycle maintenance and repair. Participants would then have the opportunity to work hours towards owning a new bicycle. This program should be primarily geared to benefit traditionally underserved communities.	Lead: United Way of Harrison County Support: Local Bicycle Shops; Gulf Coast Bicycling Club; Other Local Businesses	Long-term (10+ years)	Medium

TOPIC AREA	PROGRAM RECOMMENDATION	RESPONSIBLE PARTY	TIMEFRAME	FUNDING NEEDS
SAFETY	Create a new pedestrian/bike/motor vehicle safety campaign focused on communicating the rights of bicyclists and pedestrians to motor vehicle operators.	Lead: Gulf Regional Planning Commission Support: Municipal Police Departments; Harrison County Sheriff's Office; County and Municipal Planning Departments	Short-term (0-5 years)	Medium
	Continue Safe Routes to School programming, and explore expanding the program to additional schools.	Lead: Gulf Regional Planning Commission Support: County Board of Education	Short-term (0-5 years)	Medium
	Establish new walking & bicycle clubs that encourage physical activity in geographically and socioeconomic diversity of communities across the County, while providing safety in numbers & expertise.	Lead: County & Municipal Parks and Recreation Support: United Way; Active Living Committee; Gulf Coast Bicycle Club; Mississippi Department of Health	Mid-term (5-10 years)	Low

TOPIC AREA	PROGRAM RECOMMENDATION	RESPONSIBLE PARTY	TIMEFRAME	FUNDING NEEDS
SAFETY	Conduct Pedestrian and Bicycle-focused Road Safety Audits.	Lead: County & Municipal Public Works Support: Mississippi DOT; Municipal Police Departments; Sheriff's Office; Community Development; Local Advocates & Safe Routes to School Programs	Mid-term (5-10 years)	Medium
WAYFINDING SYSTEM	Part 1: Conduct a wayfinding study and/or wayfinding plan; deliverables will include concepts and placement plan. Part 2: Apply and obtain fabrication and installation funding for system. Part 3: Install signage and pavement markings, establish maintenance schedule.	Lead: County and Municipal Public Works Support: County and Municipal Community Development; Planning Departments	Mid-term (5-10 years)	Medium
PERFORMANCE METRICS	Create equity assessment program to determine if any transportation projects are located in traditionally underserved communities. Depending on the results of this program, re-evaluate transportation project prioritization criteria to ensure maintenance and improvement projects to historically underserved neighborhoods are prioritized appropriately.	Lead: Gulf Regional Planning Commission Support: County and Municipal Planning Departments	Mid-term (5-10 years)	Low

A person is walking a dog on a paved path that leads towards a beach. The scene is captured at sunset or sunrise, with a warm, orange glow on the horizon and a large, bright sun partially obscured by clouds. The path is made of large, light-colored paving stones. The person is wearing a light-colored shirt and dark shorts, and the dog is a medium-sized, dark-colored breed. The overall atmosphere is peaceful and active.

4

Implementation

This chapter presents an implementation strategy for the Harrison County Active Living Initiative, including Early Action Steps, Trail/Pathway Maintenance and Permitting considerations, a Funding Strategy, and Design Guidelines.

The Active Living Initiative provides a long-term vision for the development of a countywide biking and walking network that can be used by all residents for all types of trips. Implementation of the Initiative will take place in small steps over many years. This Initiative recommends that the County and cities pursue the following strategies to implement the community's vision:

- 1 Maintain implementation responsibility** by assigning the duties of an active transportation planner to a staff person within the respective agency, and/or work with the Gulf Regional Planning Commission to create a full-time active transportation coordinator for the entire Mississippi Gulf Coast.
- 2 Strategically implement infrastructure projects and programmatic initiatives** by pursuing funding for high-priority projects and programs first, and by installing projects of all priorities in conjunction with larger construction projects.
- 3 Regularly update project prioritization** to ensure that new projects are added to the list, completed projects are removed, and the priorities are revised as conditions change.
- 4 Update the Initiative** on a regular basis. Current best practice is to update the bicycle portion of transportation planning strategies every five years. Other elements of the Initiative may be reviewed and updated as needed.

5 Coordinate with neighboring counties and cities to create a safe, comfortable, and fun active transportation network across South Mississippi.

This chapter provides a roadmap for achieving the vision and goals established at the beginning of the Initiative by outlining early action steps, facility cost estimates, maintenance and permitting considerations, and potential funding sources. Harrison County and the individual cities are responsible for the implementation of active transportation infrastructure projects within their respective jurisdictions. Programs to encourage walking, bicycling, and using other active modes or to provide safety education are the responsibility of city and County departments and of regional agencies such as the Harrison County Sheriff's Office and the Gulf Regional Planning Commission.

Additionally, a safer and more active Harrison County is not possible without the involvement of community members. Our residents have priceless knowledge about the streets and trails in our community. As the County and its cities move forward with the implementation of active transportation projects, additional community engagement and outreach will be essential.

In addition, the recommendations in this Initiative should be re-evaluated at least every five years to ensure that these still constitute best practices and reflect our community's long-term vision for a safer and more active Harrison County.

Early Action Steps

Harrison County and its cities will seek to build momentum and excitement for active transportation improvements among community members by securing some key “quick wins”. The below Early Action Steps have been identified as initiatives that should be undertaken in the near term.

- **Hire a Pedestrian & Bicycle Coordinator** position to be housed at the County or Gulf Regional Planning Commission.
- **Review capital improvement and routine street maintenance projects** in the pipeline to determine if any of the recommendations in this Active Living Initiative can be added to the scope of work before implementation. In particular, consistently designed crosswalk striping should be included during any pavement rehabilitation.
- **Install “Bikes May Use Full Lane” signage** along popular rural bicycling routes throughout the County. (Figure 52)
- Create a regionally consistent policy framework for **encouraging and later requiring Complete Street improvements in new development projects** (similar to Long Beach’s sidewalk policy).
- **Create a new pedestrian/bike/motor vehicle safety campaign** focused on communicating the rights of bicyclists and pedestrians to motor vehicle operators. (Figure 53)



Figure 52 “Bikes May Use Full Lane” sign being installed along the Natchez Trace Parkway. (Photo credit: Adventure Cycling Association)



Figure 53 Sample community-inspired road safety campaign graphics



Figure 54 Conceptual vision of a potential crossing improvement along Highway 90 west of Debuys Road

- Identify locations along Highway 90 to **install enhanced crosswalks, appropriate lighting, and beacons/signals** (e.g., Pedestrian Hybrid Beacons) at priority locations based on safety and access needs. (Figure 54)

Trail/Pathway Maintenance and Permitting

It is critical that the County and cities maintain any pedestrian and bicycle facilities that currently exist or are built in the future so they remain safe and accessible for users. For instance, sand will need to be removed regularly from any beach-adjacent path to keep it usable for people using bicycles, wheelchairs, and other wheeled devices. Sidewalks and other pedestrian facilities should be

maintained to remain compliant with the Americans with Disabilities Act (ADA).

Additional considerations regarding maintenance and permitting for trails/pathways, including the recommended frequency of maintenance activities along beach pathways, are provided in Appendix B.



Figure 55 Prioritizing the cleaning of bikeways is critical to encouraging continued use of these facilities.

Funding Strategy

COORDINATION WITH OTHER AGENCIES & DEPARTMENTS

Harrison County and its cities will continue to work with adjacent communities to align priorities for projects where facilities touch boundaries. The County and cities also commit to continue integrating active transportation projects with the regional network of walkways and bikeways in partnership with local agencies and regional bodies such as the Gulf Regional Planning Commission. In addition, local agencies will coordinate with Mississippi Power to implement active transportation projects along utility rights-of-way.

Lastly, as the Mississippi Department of Transportation (MDOT) is a large funding source for active transportation projects within the state, and further maintains highways inside Harrison County's boundaries, additional coordination with this agency is important.

FUNDING SOURCES

As with many communities, Harrison County and its cities utilize regional, state, and federal funding sources to implement bicycle and pedestrian infrastructure projects and programs. Typically, these funds are distributed to jurisdictions through increasingly competitive grant processes.

Transportation funding can change drastically when there are modifications to policies and new taxes and fees are adopted.

Federal funding is perhaps the most uncertain, as the primary federal source of funding—the federal gas tax—has not been raised since 1993. Federal revenue for transportation is allocated through the federal surface transportation bill, which is developed and authorized by Congress infrequently.

The table on the following page provides a list of potential funding sources and the types of projects eligible for these funds. As the funding environment is constantly changing, many of the sources identified may be discontinued or new funding opportunities may become available. County and City staff will remain vigilant and maintain focus on adapting to secure funding from sources of revenue as opportunities arise.

LEVEL	FUNDING PROGRAM	DESCRIPTION/NOTES
Local	General Fund	The General Fund for each city and Harrison County is used in general operations of the municipality or County such as Police, Fire, Public Works, Municipal Court, Planning, Building and General Administrative business support activities
	Local Bonds	Harrison County and its municipalities have the power to issue bonds as a form of long-term debt used to build capital improvements.
	Special Local Tax	Often referred to as a Transportation – Special Purpose Local Option Sales Tax (T-SPLOST), counties and municipalities often fund transportation projects through the establishment of a specific sales tax. Passage of legislation to implement such a tax is often predicated by a ballot measure, and specific listing of projects that will be implemented with the tax funds.
State & Regional	Mississippi Department of Marine Resources Tidelands Trust Fund	Provides funds for the conservation, reclamation, preservation, acquisition, education, or the enhancement of public access to the public trust tidelands or public improvement projects as they relate to those lands. Could likely be used for pathways/walkways. Most cities and counties receive \$300,000-350,000 per year. Contact: tidelandsapp@dmr.ms.gov
	British Petroleum (BP) Deepwater Horizon Disaster Settlement Funds	Ideas for restoration projects can be submitted on a rolling basis to the Mississippi Department of Environmental Quality. There is typically \$30-50 million available each year. https://www.mdeq.ms.gov/restoration/project-portal/
	Gulf Coast Metropolitan Planning Organization Transportation Improvement Program	The Gulf Regional Planning Commission (GRPC), as the designated metropolitan planning organization for the Mississippi Gulf Coast, allocates federal Surface Transportation Block Grant (STBG) and Transportation Alternatives Program (TAP) funds to local units of government. There is a dedicated pot of funding for bicycle and pedestrian projects, and GRPC encourages the use of STBG funds to support bicycle and pedestrian projects. https://grpc.com/mpo-programs/transportation-improvement-program/

LEVEL	FUNDING PROGRAM	DESCRIPTION/NOTES
State & Regional	Gulf Coast Regional Infrastructure Program	<p>The Gulf Coast Regional Water and Wastewater Program identifies the water, wastewater, and stormwater infrastructure needs of the six Gulf Region counties of Hancock, Harrison, George, Jackson, Pearl River and Stone and provides infrastructure for long-term growth and recovery in these counties.</p> <p>https://msdisasterrecovery.com/infrastructure/gulf-coast-regional-infrastructure-program/</p>
	Local Planning Assistance Grants-Mississippi Office of Highway Safety	<p>The Mississippi Office of Highway Safety provides funding opportunities for law enforcement agencies, state agencies, community partners, colleges and universities through a grant application process. Funding is available for the following program areas:</p> <ul style="list-style-type: none"> • Alcohol (DUI-Only) • Impaired Driving (Alcohol and Drug) • Occupant Protection (Seatbelt and Child Restraint) • Police Traffic Services (Impaired Driving, Seatbelt, Child Restraint, with emphasis on Speed) • Traffic Records and Data Systems • Youth Education Programs • Adult Education Programs <p>https://www.dps.ms.gov/public-safety-planning/highway-safety/grant-application-information</p>
	Mississippi State Department of Health - STARS (Students Taking Active Routes Safely) program	<p>The Safe Routes STARS Program provides pedestrian and bicycle safety instruction to 2nd and 5th grade students, parents, schools and community members across Mississippi and will promote safe walking and biking as fun, healthy ways to get to and from school safely. The program is available to all school districts in the state of Mississippi.</p> <p>https://msdh.ms.gov/msdhsite</p>

LEVEL	FUNDING PROGRAM	DESCRIPTION/NOTES
Federal	Safe Streets and Roads for All	<p>This new federal program is funded at \$5B appropriated (and up to \$6B total pending future appropriation) for five years and is a competitive program for local governments to create Safety Action Plans (Vision Zero) and to fund the recommendations from these plans. Eligible activities include developing a comprehensive safety (or Vision Zero) action plan, conducting design and development activities in support of the Action Plan, and carrying out projects and strategies identified in an Action Plan. Given the completion of the Harrison County Active Living Plan in 2022, this is ideal timing to fund projects using local funds as match.</p> <p>https://www.transportation.gov/SS4A\</p>
	Rebuilding American Infrastructure with Sustainability and Equity (RAISE)	Formerly known as TIGER and BUILD under previous administrations, the RAISE program is funded at \$7.5B over the five year period (starting in 2021). This is a popular competitive program that funds local or regional transportation projects that improve safety, environmental sustainability, quality of life, economic competitiveness, state of good repair, and connectivity. In 2021, multi-modal projects (that included active transportation elements) made up over 80% of the awarded projects.
	The Active Transportation Infrastructure Investment	This is a competitive program for Active Transportation networks and spines, with \$1B authorized but not yet appropriated as of spring 2022. This program would focus on improvements addressing walking and bicycling infrastructure.
	Reconnecting Communities	<p>This program has \$500M appropriated (and up to \$1B total pending future appropriation) for planning and construction to remove mobility barriers in communities impacted by past infrastructure implementation. This may address Vision Zero issues, particularly where a major highway has divided communities, making for dangerous roadway crossings.</p> <p>https://www.transportation.gov/grants/reconnecting-communities</p>
	Healthy Streets	This is a \$500M discretionary program that will address issues such as urban heat island/tree cover in low income and minority communities. This funding source may address Vision Zero concerns by creating streetscapes and other measures to calm or slow traffic.

LEVEL	FUNDING PROGRAM	DESCRIPTION/NOTES
Federal	The Carbon Reduction Formula Program	<p>This is a \$6.42B program that will be allocated by population to states and then downwards to municipalities. Eligible projects include public transit projects, bikeways, walkways, and trails to facilitate non-motorized users of the road.</p> <p>https://www.fhwa.dot.gov/bipartisan-infrastructure-law/crp_fact_sheet.cfm</p>
	United States Department of Transportation (USDOT) Federal Transit Administration (FTA) Section 5310 Non-traditional uses	<p>FTA Section 5310 funds are discretionary grants for the purpose of assisting private nonprofit groups in meeting the transportation needs of older adults and people with disabilities when the transportation service provided is unavailable, insufficient, or inappropriate to meet these needs. Non-traditional Section 5310 projects include building an accessible path to a bus stop, including curb-cuts, sidewalks, accessible pedestrian signals or other accessible features as well as improving signage or wayfinding technology.</p>
	USDOT Federal Highway Administration (FHWA) Congestion Management and Air Quality (CMAQ) Improvement Program grants	<p>The FAST Act continued the CMAQ program to provide a flexible funding source to State and local governments for transportation projects and programs to help meet the requirements of the Clean Air Act. Funding is available to reduce congestion and improve air quality for areas that do not meet the National Ambient Air Quality Standards for ozone, carbon monoxide, or particulate matter (nonattainment areas) and for former nonattainment areas that are now in compliance (maintenance areas).</p>
	National Park Service Land and Water Conservation Fund (LWCF) State and Local Grant Program	<p>Launched by Congress in 1964, the LWCF was designed to direct revenue from federal off-shore oil and gas leases (not taxpayer dollars) to acquire land and conservation easements to protect our national parks, wildlife refuges, forests and trails. Through the Great American Outdoors Act of 2020, the federal government fully and permanently funded the LWCF at \$900 million annually.</p> <p>https://www.mdwfp.com/parks-destinations/outdoor-recreational-grants/lwcf-faqs/</p>

LEVEL	FUNDING PROGRAM	DESCRIPTION/NOTES
Federal	USDOT FHWA National Recreational Trails Program (RTP)	<p>The RTP provides funds to the States to develop and maintain recreational trails and trail-related facilities for both non-motorized and motorized recreational trail uses.</p> <p>https://www.fhwa.dot.gov/environment/recreational_trails/</p>
	USDOT FHWA Transportation Alternatives Set Aside	<p>The Fixing America's Surface Transportation (FAST) Act replaced the former Transportation Alternatives Program (TAP) with a set-aside of funds under the Surface Transportation Block Grant Program (STBG). This funding is intended for programs and projects defined as transportation alternatives, including on- and off-road pedestrian and bicycle facilities, infrastructure projects for improving non-driver access to public transportation and enhanced mobility, community improvement activities such as historic preservation and vegetation management, and environmental mitigation related to stormwater and habitat connectivity; recreational trail projects; safe routes to school projects; and projects for planning, designing, or constructing boulevards and other roadways largely in the right-of-way of former divided highways.</p> <p>https://www.fhwa.dot.gov/environment/transportation_alternatives/</p>
	Centers for Disease Control Racial and Ethnic Approaches to Community Health (REACH)	<p>CDC funds 40 recipients to reduce health disparities among racial and ethnic populations with the highest burden of chronic disease, such as hypertension, heart disease, type 2 diabetes, and obesity. The recipients work through culturally tailored interventions to address preventable risk behaviors, including tobacco use, poor nutrition, and physical inactivity.</p>
	Safe Routes to School, through MDOT's Transportation Alternatives Program	<p>Construction of pedestrian, bicycle transportation, and traffic calming facilities near schools. Funds can also be used for non-infrastructure programs that support walking and bicycling to/from schools.</p> <p>Contact: Paula Morgan, Mississippi; TAP Coordinator; (601) 359-7222.</p> <p>http://mdot.ms.gov/safetyeducation/programs/safe-routes-to-school/details.aspx</p>

LEVEL	FUNDING PROGRAM	DESCRIPTION/NOTES
Federal	National Park Service's Mississippi Gulf Coast National Heritage Area matching grants	<p>To help accomplish the goals and objectives of the Management Plan, MGCNHA offers matching grants to provide and leverage funding for innovative projects across the region that preserve, interpret and develop heritage resources that expand economic opportunity. Funding for the grant program is provided by the federal government through the National Park Service.</p> <p>https://dmr.ms.gov/mgcnha/</p>
Private	Healthy School Grant Awards & Health Hometown Grant Awards	<p>Blue Cross & Blue Shield of Mississippi Foundation Healthy School Awards and Healthy Hometown Grants</p> <p>Up to four grants are awarded in each cycle to communities that demonstrate a commitment to improving the health and wellness of their stakeholders, with the funds helping the municipality continue or expand successful efforts. Grants range from \$25,000 to \$50,000. An organization must submit a Letter of Inquiry to begin the grant application process. Based on the information in the Letter of Inquiry, the Foundation decides whether to request a full grant application. The Foundation accepts Letters of Inquiry throughout the year.</p> <p>https://www.healthiermississippi.org/healthy-awards/hometown/</p>

Design Guidelines

Various pedestrian and bicycle design guidelines present tools for creating a pedestrian- and bicycle-friendly, safe, and accessible community. These guidelines are not a substitute for a more thorough evaluation by a professional upon implementation of facility improvements, but instead offer an overview of best practices established across the nation. The design guidelines and recommendations listed are not intended to replace existing state or national mandatory or advisory standards nor the exercise of engineering judgment by licensed professionals, but will instead help inform the County's and Cities' decisions when evaluating new projects. National and state design guidance and details can be found in the following documents.

NATIONAL GUIDANCE

The American Association of State Highway and Transportation Officials (AASHTO) Guide for the Planning, Design, and Operation of Pedestrian Facilities (2004) provides comprehensive guidance on planning and designing for people on foot and using other mobility devices such as wheelchairs.

Offering similar guidance for bicycle facility design, the AASHTO Guide for the Development of Bicycle Facilities (2012) provides guidance on dimensions, use, and layout of specific bicycle facilities.

The National Association of City Transportation Officials' (NACTO) Urban Street Design Guide (2013) is the most recent publication

of nationally recognized urban street design standards, and offers guidance on the current state of the practice designs.

AASHTO's A Policy on Geometric Design of Highways and Streets (2011), commonly referred to as the "Green Book," contains current design research and practices for highway and street geometric design.

FHWA's Small Town and Rural Multimodal Networks Guide (2016) is a resource to help small towns and rural communities support safe, accessible, comfortable, and active travel for people of all ages and abilities. It provides an overview of bicycle and pedestrian designs for these communities, as well as examples of peer communities. (Figure 56)

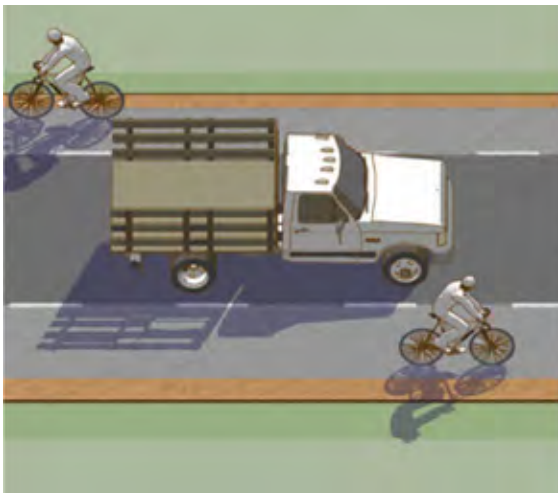
STATE GUIDANCE

MDOT provides several design specifications and manuals for roadway and bridge projects on their website:

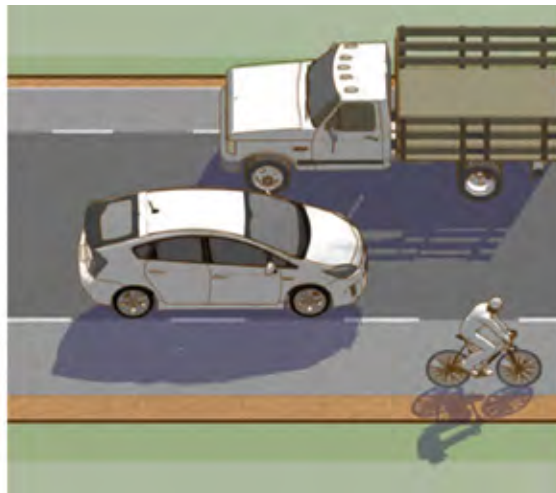
https://mdot.ms.gov/portal/engineering_standards_guides_manuals

In addition, standards and typical section drawings for a selection of pedestrian and bicycle facilities are provided in Appendix C.

10 ft (3.0m) Center Travel Lane



13 ft (4.5m) Center Travel Lane



18 ft (15.5m) Center Travel Lane

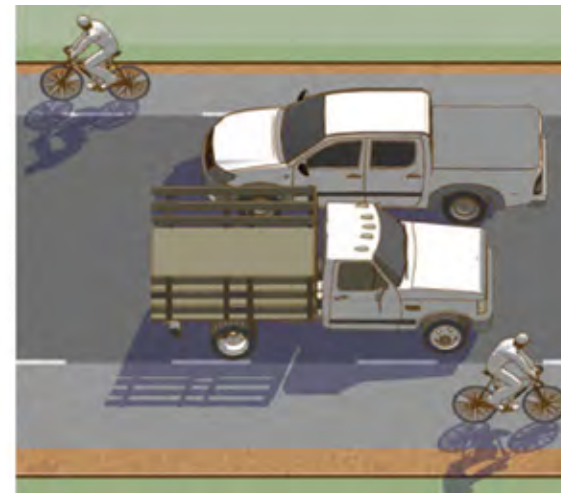
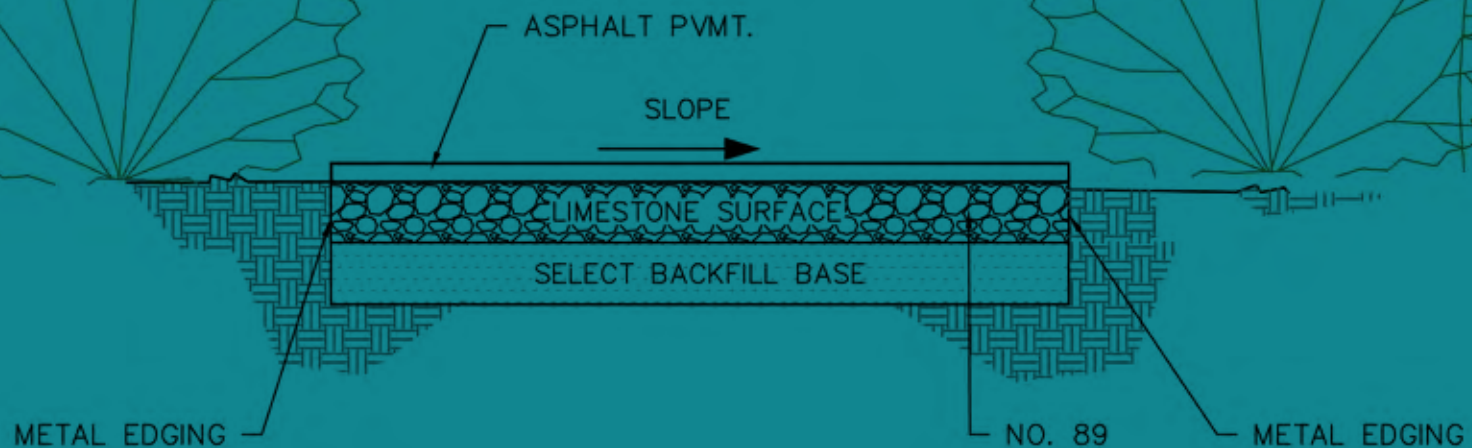


Figure 56 Advisory Shoulder diagrams from the FHWA Small Town and Rural Multimodal Networks Guide

APPENDIX

A

Current Plans and Policies



Current Plans and Policies

MULTIPLAN 2045: MISSISSIPPI'S UNIFIED LONG-RANGE TRANSPORTATION INFRASTRUCTURE PLAN (2020)

This plan covers transportation trends and budget scenarios for the state of Mississippi. MDOT assets are laid out and analyzed for required and visionary funding. These assets include pavement conditions, bridges, capacity, safety, railroads, etc. MDOT data addresses several different transportation statistics, including the fact that 65% of statewide traffic fatalities occur on the state highway system, and that transit ridership in rural communities has increased more rapidly than urban communities since 2015.

For bicycle and pedestrian facilities, level of comfort was measured for the 5 existing statewide bicycle routes. The Southern Tier bicycle route that runs through Harrison County is mostly rated at moderately high comfort or above. MDOT plans to allocate at least \$1M annually for bicycle and pedestrian infrastructure until 2045. However, \$180M is the bare minimum investment needed by 2045 and \$426M is an aggressive goal which includes all MPO proposed projects and closes gaps in the trail network. MDOT also recommends maintaining the trend of allocating \$30M annually for safety investments until 2045. The minimum amount of investment for safety by 2045 is \$780M.

Multiplan 2045 key takeaways

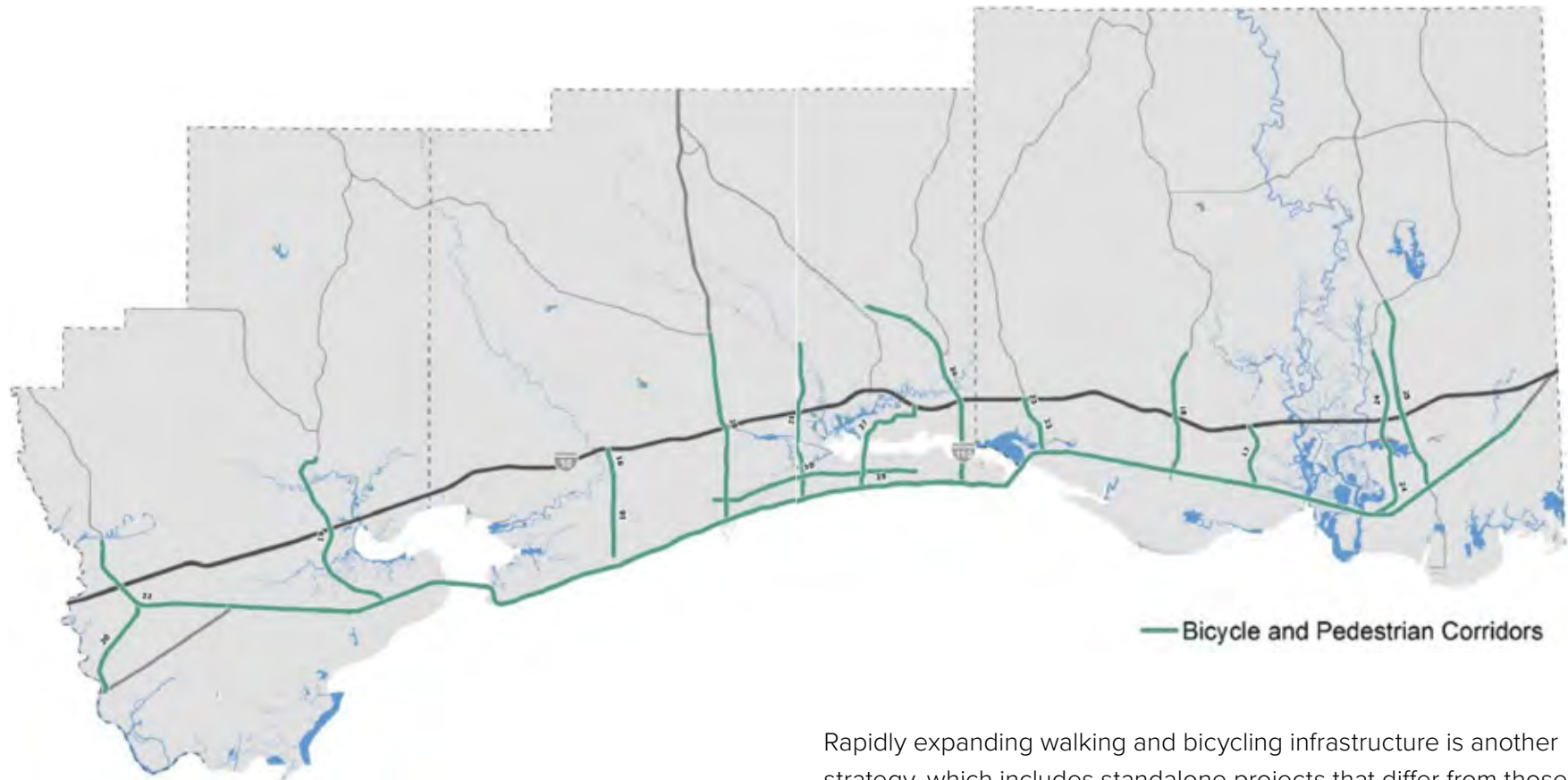
Funding for bicycle and pedestrian facilities will largely need to rely on sources outside of MDOT.

Safety resources may be a more readily available source of MDOT funding.

The Southern Tier bicycle route is considered a relatively comfortable facility.

2045 METROPOLITAN TRANSPORTATION PLAN FOR THE MISSISSIPPI GULF COAST METROPOLITAN PLANNING ORGANIZATION (2020)

The 2045 Metropolitan Transportation Plan covers the Mississippi counties of Harrison, Hancock, and Jackson. The plan recognizes that driving, both in necessity and desire, is declining. Alternative transportation will be needed to accommodate the large aging population of the county, among other factors for our changing world. The GRPC has six goals for the region, including improving and expanding transportation choices, improving safety and security, providing a reliable and high performing transportation system, and managing the relationship of transportation, community, and environment.



The plan identifies a set of strategies for improving transportation in the region. The first strategy includes responsibly improving the roadway system. Widening projects are considered to be rare and highly calculated for cost-benefit. A prioritized list of these new roadways or widening projects can be found in the document.

Rapidly expanding walking and bicycling infrastructure is another strategy, which includes standalone projects that differ from those included in new roadway projects. The plan identifies fiscally constrained projects for the region, as well as visionary project recommendations (as seen below). For the next two years, the plan recommends seeking out funding for high-priority bicycle and pedestrian projects.

Several transit projects are underway, including a new bus route as well as restoring the Sunset Limited Amtrak Route.

2045 Metropolitan Transportation Plan key takeaways

The Gulf Regional Planning Commission hopes to tie together the region with forward thinking transportation systems that focus on public transit and walking and biking options.

Several visionary bicycle and pedestrian projects were identified for the region.

Between 2010 and 2020, Harrison County in particular increased in population by 11 percent, and surrounding counties had increases in population.

The 2045 plan recognized that an aging population, reduced number of trips overall and increased interest in car-free or car-lite lifestyles will increase the need for investments in bicycle and pedestrian options.

D'IBERVILLE SIDEWALK AND PEDESTRIAN STUDY (2020)

The D'Iberville Sidewalk and Pedestrian Study focuses on providing recommendations to the City which will build pedestrian connections to schools, parks, public facilities, transit stops and workplaces; improve the walkability of business districts; infill missing portions of the pedestrian network; improve facilities currently in poor condition. The study finds that pedestrian demand is highest in the areas around D'Iberville Middle School, along major commercial corridors including Lamey Bridge Road and D'Iberville Boulevard, and the commercial development north of I-10.

The recommended network suggests completion of facilities along the three primary corridors: Auto Mall Parkway, Lamey Bridge Road, and D'Iberville Boulevard; east-west connections across I-110, including Rodriguez Street and Popps Ferry Road; shared-use facilities anchored by a new facility proposed for the Mississippi Power utility easement; and sidewalk connections to nearby residential districts. Overall, the plan recommends \$19.2 million in pedestrian facilities, \$8.5 million of which is a near-term Tier 1 pedestrian projects. The plan also recommends 34 crossing improvements. These proposed projects are shown in Figure 4.

In addition to infrastructure improvements, the plan also recommends that the City of D'Iberville adopt a Complete Streets policy, require sidewalks during new construction or redevelopment, and require connectivity cross-access between adjacent land parcels.

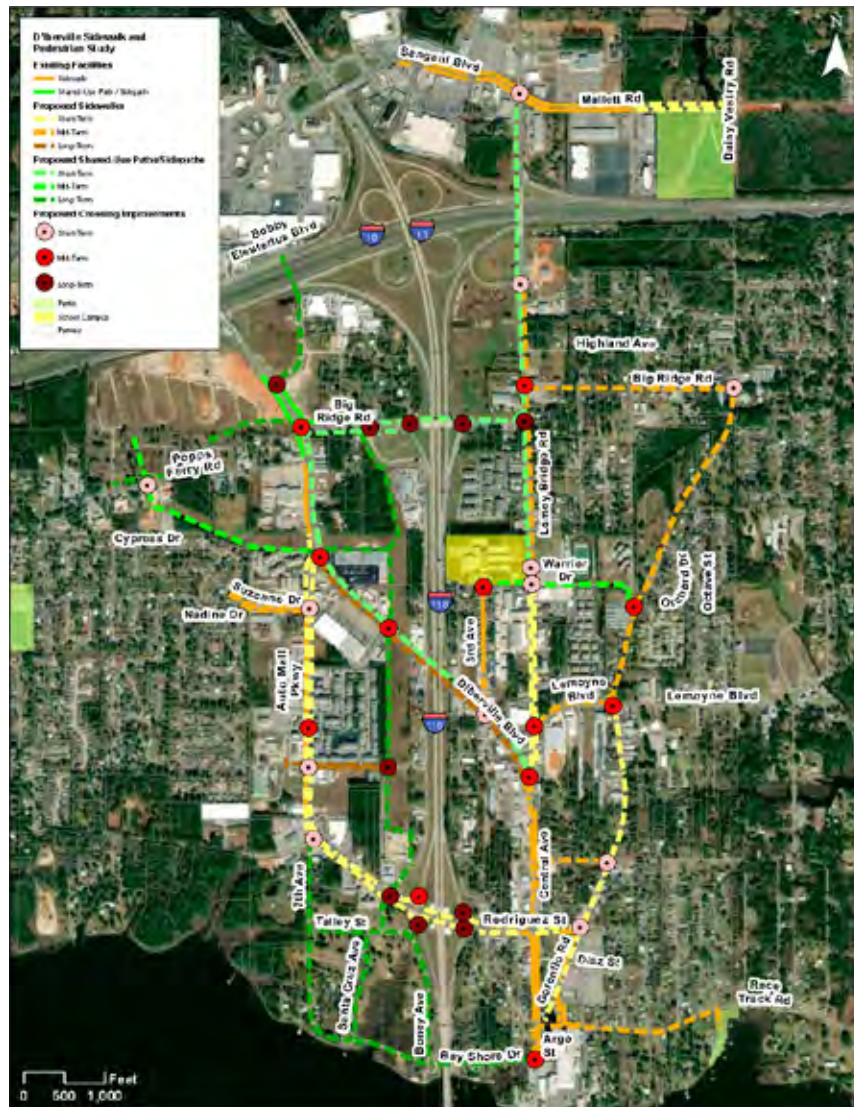


Figure 57 Scheduled pedestrian capital improvements in D'Iberville

PEDESTRIAN STUDY FOR THE US HIGHWAY 49 CORRIDOR (2020)

This study examined the one-mile corridor along US Highway 49 between Creosote Road and Turkey Creek in Gulfport. Currently there are no bicycle or pedestrian facilities along the corridor, but residents of Gulfport are walking and bicycling in the area often with approximately 60-70 bicyclists or pedestrians observed during one 11-hour study period. Safety is a major concern, as there have been 22 crashes between bicyclists/pedestrians and vehicles along the corridor since 2012; five of these crashes resulted in the death of a bicyclist or pedestrian.

This section of US Highway 49 is designed to serve vehicular traffic from nearby I-10 and incorporates multiple commercial uses: including lodging, restaurants, and major retail shopping centers. The study recommends a few interventions to improve bicyclist and pedestrian facilities: installation of a sidepath along the highway; installation of crosswalks, pedestrian signals, median refuges, and ramps at intersections; reassessment and improvements to lighting; and the installation of a center median barrier to encourage pedestrian crossings at intersections.

The Highway 49 corridor study also recommends a new mid-block crossing for pedestrians in front of the Wal-Mart shopping center using a pedestrian hybrid beacon. This is key as the two existing intersections are approximately one-third of a mile apart. The study also recognizes that some intersection turning movements could

be consolidated, allowing for shorter wait times for pedestrians and bicyclists. Installing lagging left-turn traffic signal phases at the existing intersections is also recommended to enable bicyclists and pedestrians to start crossing before the turning movements. The total recommended improvements in the study were estimated to cost approximately \$4.2 million.

DOWNTOWN GULFPORT MASTER PLAN (2018)

Downtown Gulfport has undertaken a large amount of development in the past 14 years. This 2018 plan is a follow-up to a revitalization master plan developed in 2006, and encourages the continued efforts of the first plan.

The report mentions key streets as physical assets for downtown, including 14th Street and 25th Avenue as primary cross streets and Beach Blvd as a continuous east/west connection. The plan identifies strong activity zones in the downtown area along 25th Avenue, 14th Street, 13th Street, and Fishbone Alley. Accessibility challenges downtown include: poor connections between activity zones; presence of megablocks; access to the private street 27th Avenue; the perceived barrier of the railroad; some areas with minimal shade; and an underperforming gateway at 25th Street and Pass Road. In addition, the lack of access across Beach Boulevard to Jones Park was particularly highlighted.

The Downtown Gulfport Master Plan recommends: dividing the existing megablocks into a more walkable street grid; enhancing the streetscape and connections along Beach Boulevard; and recommends focusing land uses in the area that will activate Jones Park such as additional housing attractive to families, as well as restaurants and retail. The plan also highlights the importance of wayfinding as well as investing in a parking deck.

Downtown Gulfport Master Plan key takeaways

Phase III streetscape plans have yet to be completed and key streets have been identified.

A proposed pedestrian walkway/trail is suggested along Beach Boulevard to interact with Jones Park.

This plan proposed more shade trees and streetscape amenities along with the new infrastructure.

The railroad is currently a barrier to the West side of downtown, even though it has crossing locations.

The report highlights how phasing can be used to achieve these goals. The plan also shows conceptual diagrams for several nodes of development opportunities and new districts that are being implemented, such as the Aquarium District, Trackside Innovation District, Gulfside District, and Westside District.



Figure 58 Proposed Phase III Infrastructure Improvements for Downtown Gulfport

GULFPORT PEDESTRIAN SIDEWALK MASTER PLAN (2015)

The City of Gulfport developed this sidewalk master plan in order to encourage walking in this city, which will become an overall positive for the health of city residents, transportation, accessibility, and environmental responsibility. This master plan utilizes a scoring technique combining roadway classification with popular destinations. Overall, the plan provides a prioritized list of 484 new sidewalk projects throughout the city, and estimates that new sidewalks will cost \$27 per foot.

Gulfport Sidewalk Master Plan key takeaways

The City of Gulfport's 50/50 Sidewalk Program is a local initiative that encourages community buy-in of new sidewalks.

The top 99 prioritized new sidewalk projects are all along principle arterial roadways .

A total of 29 of the top 34 prioritized projects are along Pass Road.

COMMUNITY PLAN FOR WESTERN HARRISON COUNTY (2008)

Western Harrison County has shaped its community around its natural resources. As of 2008, about 74% of land in this area is forest. The community intends for its future to focus on the preservation of their natural resources as well as creating connections to enjoy these resources.

Growth in Western Harrison County is projected to double by 2030 due to thousands of new housing units being developed north of US Interstate 10 and west of US Highway 49. Residents found that walkways and bike paths are undesirable near roadways, and more desirable as natural paths.

Community Plan for Western Harrison County key takeaways

Residents of these communities would like to see greater access to recreational trails in the forested areas of the county, as well as access to water.

The Wolf River is seen as a great opportunity for conservation and public access, and a greenway is recommended along the river.

Closer access to parks, remediating brownfield sites and Safe Routes to Schools are priorities of the community.

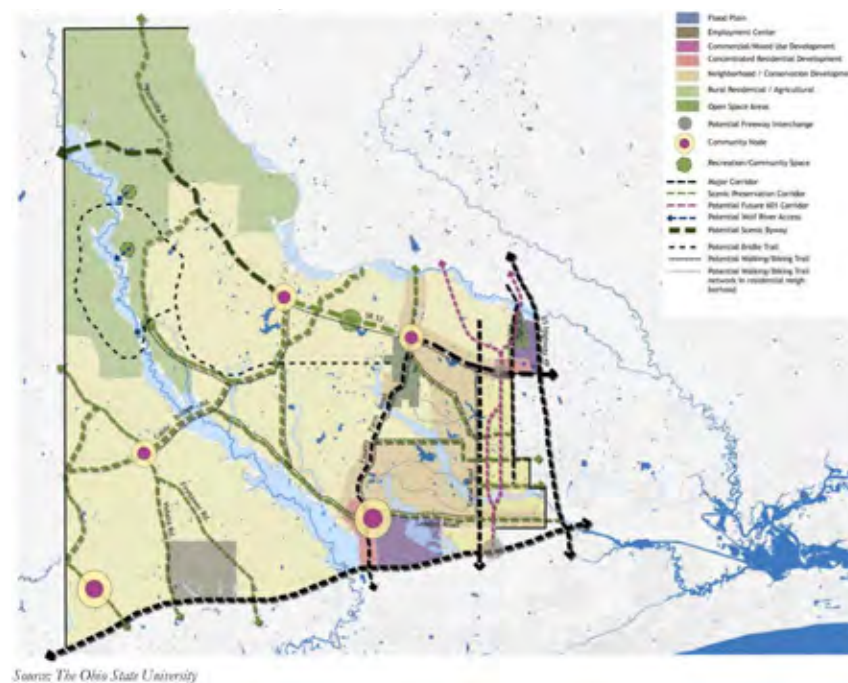


Figure 59 Western Harrison County Concept Plan

COMMUNITY PLAN FOR DELISLE (2006)

This plan was largely developed out of response to the devastation caused by Hurricane Katrina. The DeLisle community intended for this plan to serve as guidance for future rebuilding, growth, and preservation of the community's character. Much of the plan identifies community concerns and values for the future, along with a snapshot of community data at the time.

As of 2006, DeLisle had a few sidewalks and no bike paths. According to the 2006 Community Plan, traffic increased significantly post Hurricane Katrina. The increased traffic led to safety concerns with walking to school, resulting in more vehicular transportation. The only walking track within the community is used as a point of vehicular distribution.

Recommendations for the community of DeLisle are largely zoning and land use focused. The plan identifies recommended areas for higher density versus lower density housing, preservation areas, and a local bike path that engages with the heart of the community. The plan also presents different architectural housing types and neighborhood layout schemes with preservation and hazard mitigation efforts in mind.

Outdoor spaces are also mentioned in various ways, including the reduction of impervious areas, incorporating natural plants in landscapes, and the community's desire for more recreational facilities. Two sites were recommended for the implementation of a public park, including the state-owned land near the bayou and

the land adjacent to the Old DeLisle Cemetery. The community has large support for a community recreation center that can be transformed into a hurricane refuge shelter.

Community Plan for DeLisle key takeaways

DeLisle residents want to see more public access to nature as well as safe walking infrastructure.

Many residents mentioned the need of transportation for elderly residents.

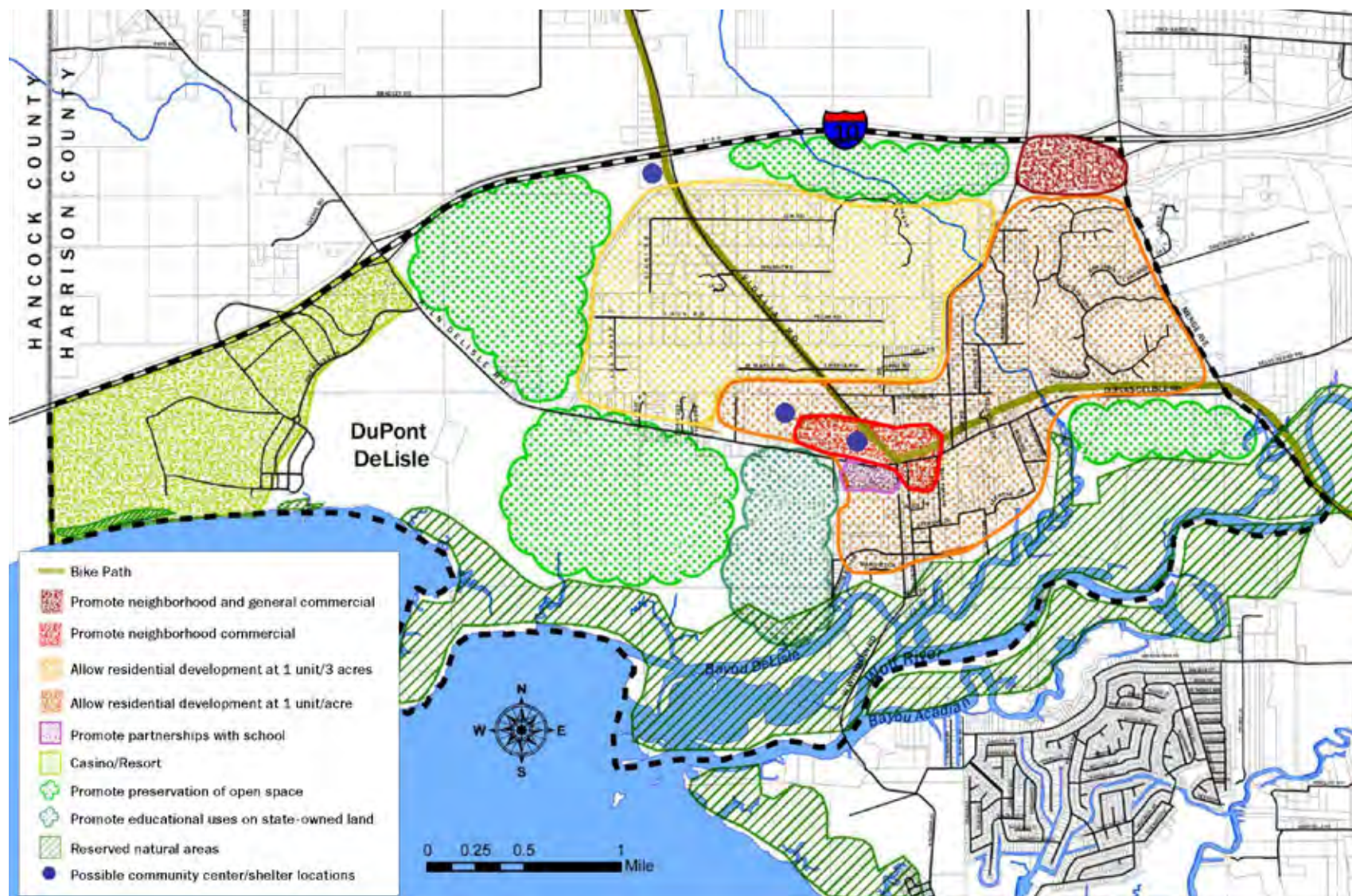
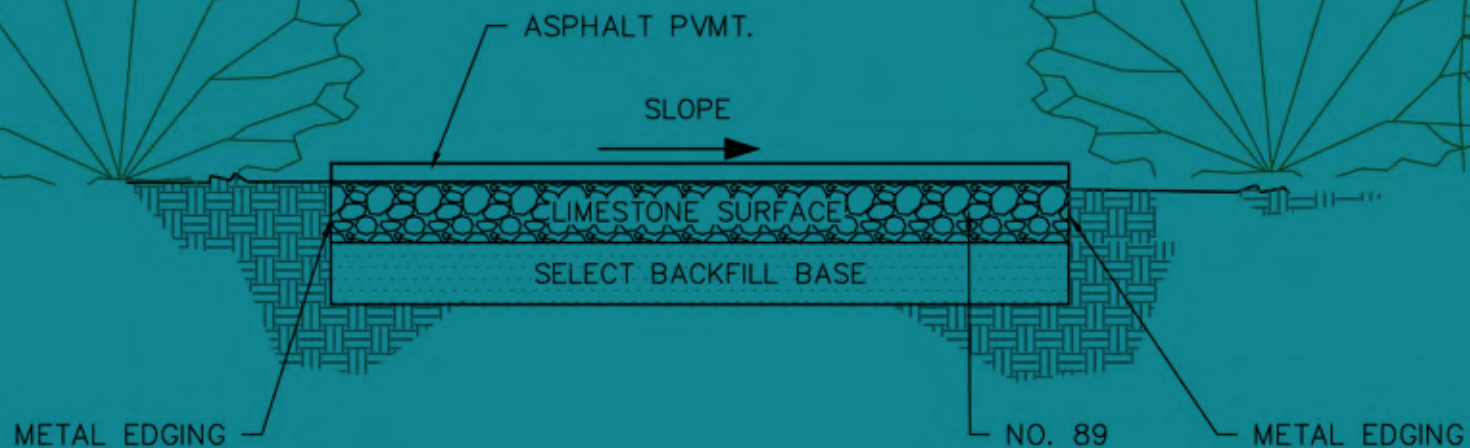


Figure 60 Comprehensive Policy Map for DeLisle, including proposed bike paths

APPENDIX **B** Maintenance and Permitting



Maintenance Considerations

1. CONCRETE PATHWAYS ON THE BEACH

Concrete pathways are generally located within a FIRM V (wave velocity) zone. These seawall-supported concrete sidewalks are designed to withstand coastal wave loading up to a Category 3 hurricane. Regular maintenance will consist of the cleaning and sealing of the pathway's joints on a 5-year basis, but overall concrete sidewalks have proven to be relatively maintenance-free. Seawall supported concrete sidewalks have an estimated useful life of 75 years or more with consideration to hurricanes and tropical storms. Major maintenance after storms may include the replacement of timber beach access stairs, ancillary signage, and timber benches.

2. TIMBER PATHWAYS ON THE BEACH

The timber beach pathways have two (2) designs, 1) Sacrificial Decking design and 2) the Thru Flow decking. Both designs are made up of timber piling and timber framing. Common in Harrison County, the sacrificial decking design uses timber or synthetic wood decking, while the Thru Flow decking design uses more resistant thru flow composite material decking, which reduces the uplift force of waves on the structure.

Timber decking pathways located on the beach are relatively new to Harrison County. Based on experience with piers made of similar materials the timber pilings will have a useful life of over 30 years, and the timber framing a useful life of 15 years. The timber framing's shorter life span is primarily a result of storm damage, not rotting. Maintenance for the timber framing will include inspection every two (2) to three (3) years or after any major storm.

The sacrificial decking design uses timber or synthetic wood decking that should anticipate damage by a storm within ten years. To maintain the sacrificial decking design, anticipate replacing up to 50% of decking after a tropical storm or minor hurricane. Thru Flow decking is not yet widely used in Harrison County, however, it is expected to be more storm-resistant and has an expected life of approximately 25 years, possibly more. Maintenance for composite Thru Flow decking would require tightening screws and fasteners every 1-2 years.

3. INLAND PATHWAYS AND TRAILS (GRASS, LIMESTONE, ASPHALT, CONCRETE)

Grass pathways should be appropriately designed to shed water runoff. The maintenance of grass pathways would include mowing in the warmer months, regular watering, and base repairs when soft spots form. Well-used paths can be beaten down and become bare; in those cases, it may be better to consider a limestone path as an alternative.

Limestone (or crushed concrete) pathways would require very little maintenance if constructed correctly. Properly designed limestone pathways would include three (3) to four (4) inches thick limestone on a compacted base and would be designed to shed water runoff. The only maintenance the pathway would require would be limited to grading and providing a supplemental limestone fill every few years.

Asphalt pathways would include granular (limestone or crushed concrete) base and a minimum course of asphalt pavement.

Asphalt pathways could be the lowest maintenance option if adequately designed to shed runoff. Routine maintenance for pathway features like replacing ancillary signs may be required, but the asphalt pathway itself could have a life of up to 25 years. It is anticipated that little maintenance would be required as long as the pathway is used for pedestrian use.

Concrete pathways include roadside sidewalks and cross-country paths to bridge areas where the soil is unsuitable and needs

more durability. Properly designed concrete pathways could be non-reinforced concrete having a thickness of four (4) inches and constructed with proper joints on a prepared well-drained base. Maintenance would include the clearing and sealing of joints every five years. Inland concrete paths can last up to 75 years or more if they are not subjected to vehicular use or were accommodated in thickness for vehicular use.

4. INLAND TIMBER BRIDGES/PATHS

Inland timber bridge pathways could be constructed in areas subjected to flooding but not wave velocity. Inland timber bridges and paths are built using timber pilings and timber frames like coastal timber pathways. The decking can be made up of timber or composite decking. Composite thru flow decking would make the pier more hazard resistant if the pathway were exposed to uplift forces, but uplift conditions are uncommon in inland settings. No matter the material the decking is made up of, it will have a longer life than coastal timber pathways.

The useful life of the timber pilings would be approximately 40 years, the timber framing approximately 25 years, and the decking approximately 15 years. The maintenance needed for inland timber decking and bridges would require an inspection every 3 years for the deck board and an inspection every three (3) to five (5) years for the framing to check for sporadic members that may have been damaged.

Permitting Summary

1. CONCRETE PATHWAYS ON THE BEACH

There is no permitting required by the US Army Corp of Engineers (USACE), Mississippi Department of Marine Resources (MDMR), or Tideland's lease to construct concrete pathways on the beach. Only local building permitting will be required, and it can vary from city to city. However, because the USACE constructed the seawall, it is common practice to have an informal concurrence with the USACE to ensure that the work being done would not be viewed as detrimental to the seawall.

2. TIMBER PATHWAYS ON THE BEACH

General design restraints for timber pathways on the beach are to avoid existing dunes and dune vegetation. Recent experience suggests that these projects would typically be waived of any permits. An example is the West Biloxi Board Walk.

3. INLAND PATHWAYS

Inland pathways are grass, limestone, asphalt, and concrete pathways. A pathway that located along or across a state highway would require Mississippi Department of Transportation (MDOT) permitting which can be reasonably obtained.

As a group, these pathways will be subject to regulations and permit requirements by the U.S. Army Corp of Engineers (USACE)

and the Mississippi Department of Marine Resources (MDMR). To design pathways in wetland areas, reasonable action to avoid, minimize, and mitigate wetland impact must be demonstrated. Reasonable action is best accomplished by providing proof of other alternatives accompanied by an explanation of why the alternatives are not suitable or reliable.

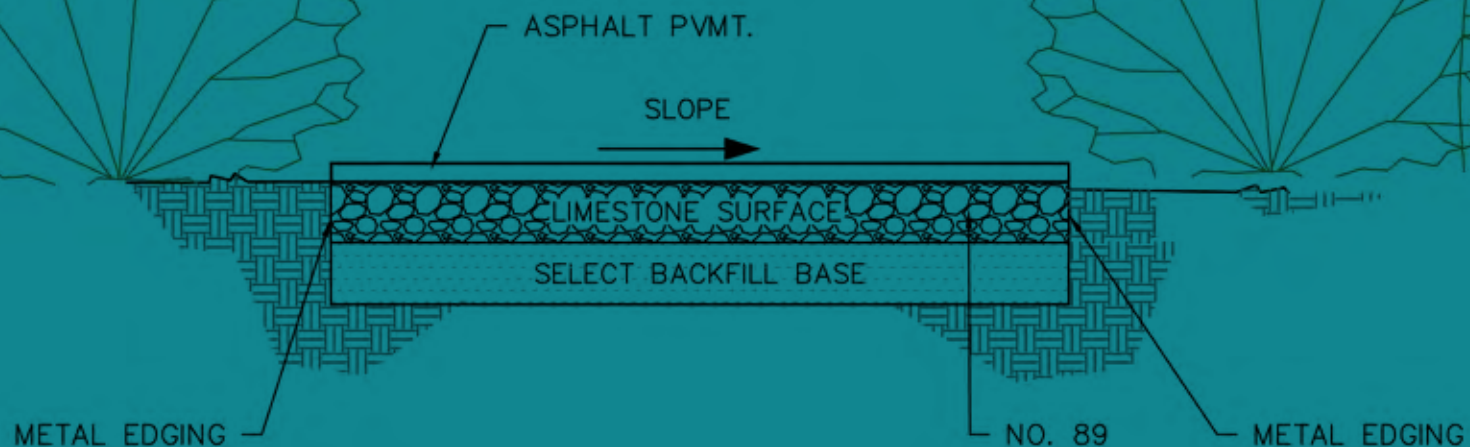
A Nationwide Permit from the US Army Corp of Engineers (USACE) would likely be available to most inland pathways that impact a small portion of wetlands. It is recommended to design these pathways in upland areas to avoid USACE and MDMR regulatory permitting and to use timber bridges in wetland areas to minimize wetland fill.

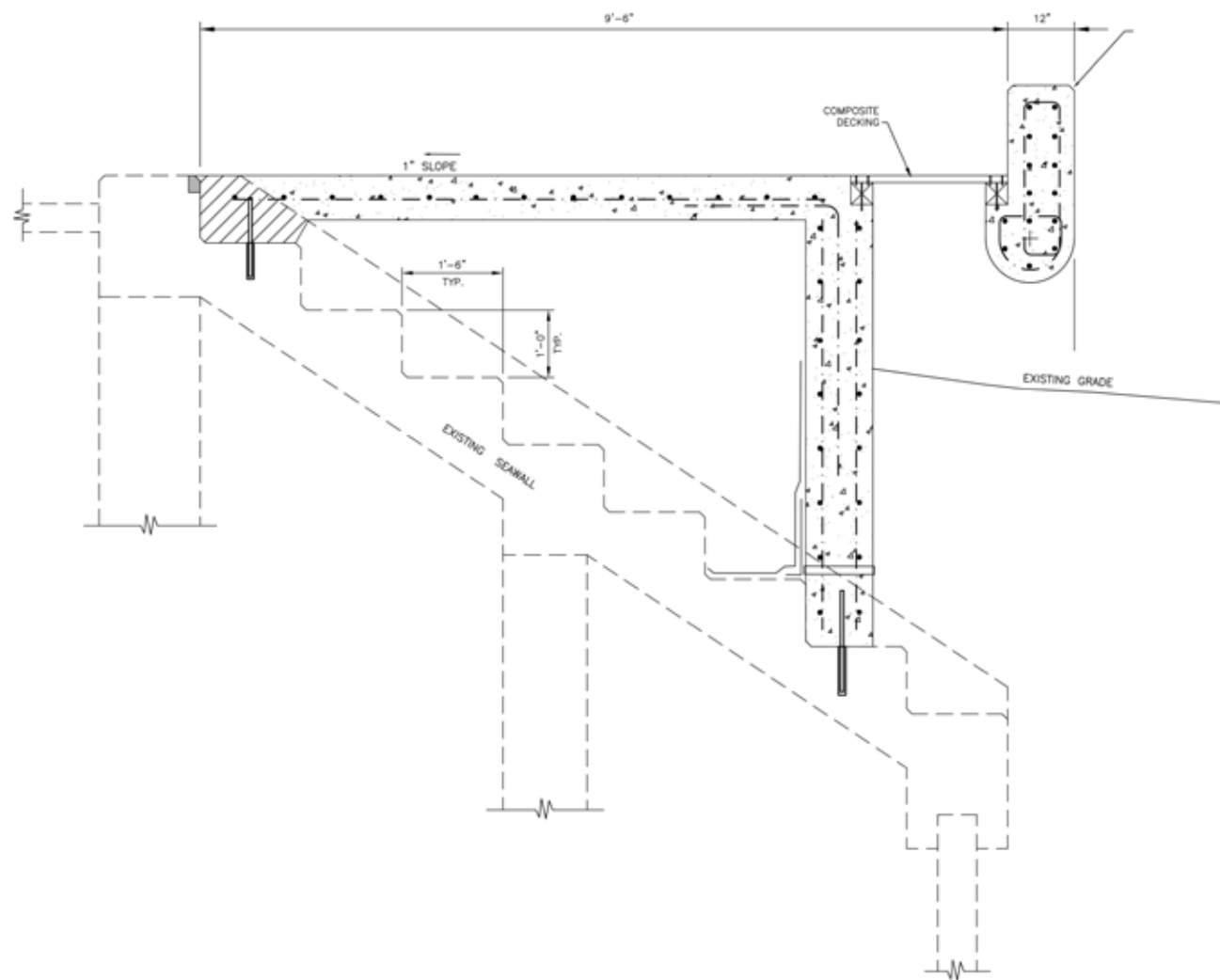
4. TIMBER BRIDGE

Anticipate constructing timber bridges where pathways cross bayous, estuaries, wetlands, and deep ditches. In most cases permitting is not required to build bridges because pilings are not considered fill in a wetland. However, it is common practice to notify USACE and the Mississippi Department of Marine Resources of any work being done inside a wetland area.

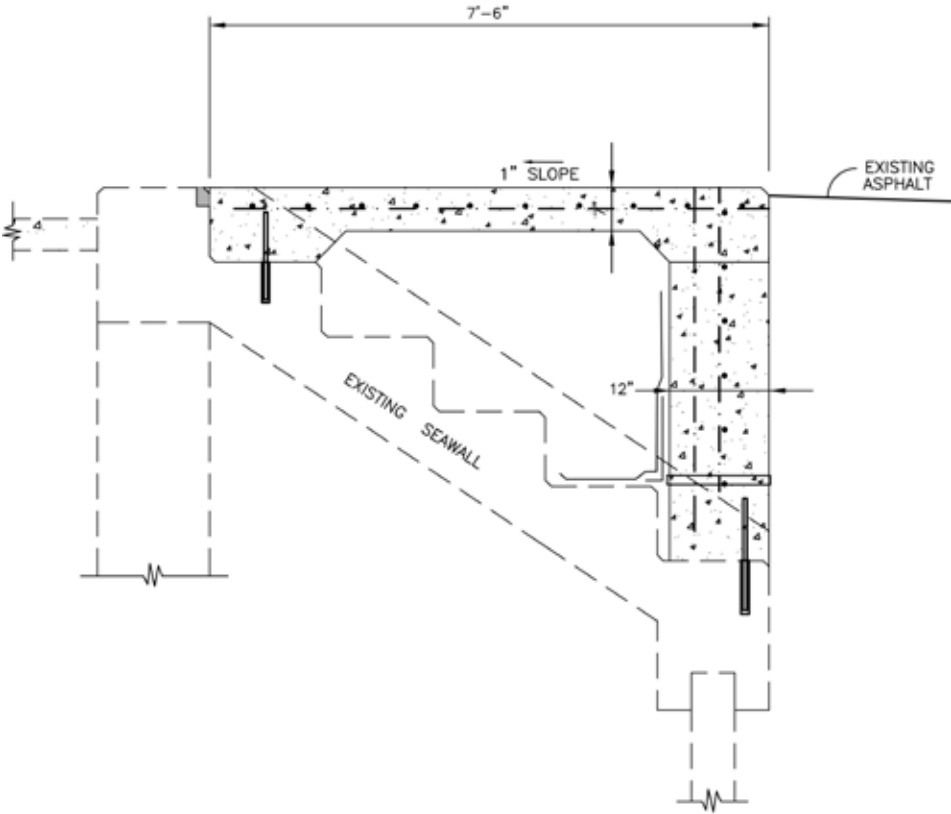
APPENDIX C

Standards and Typical Section Drawings for Pedestrian and Bicycle Facilities

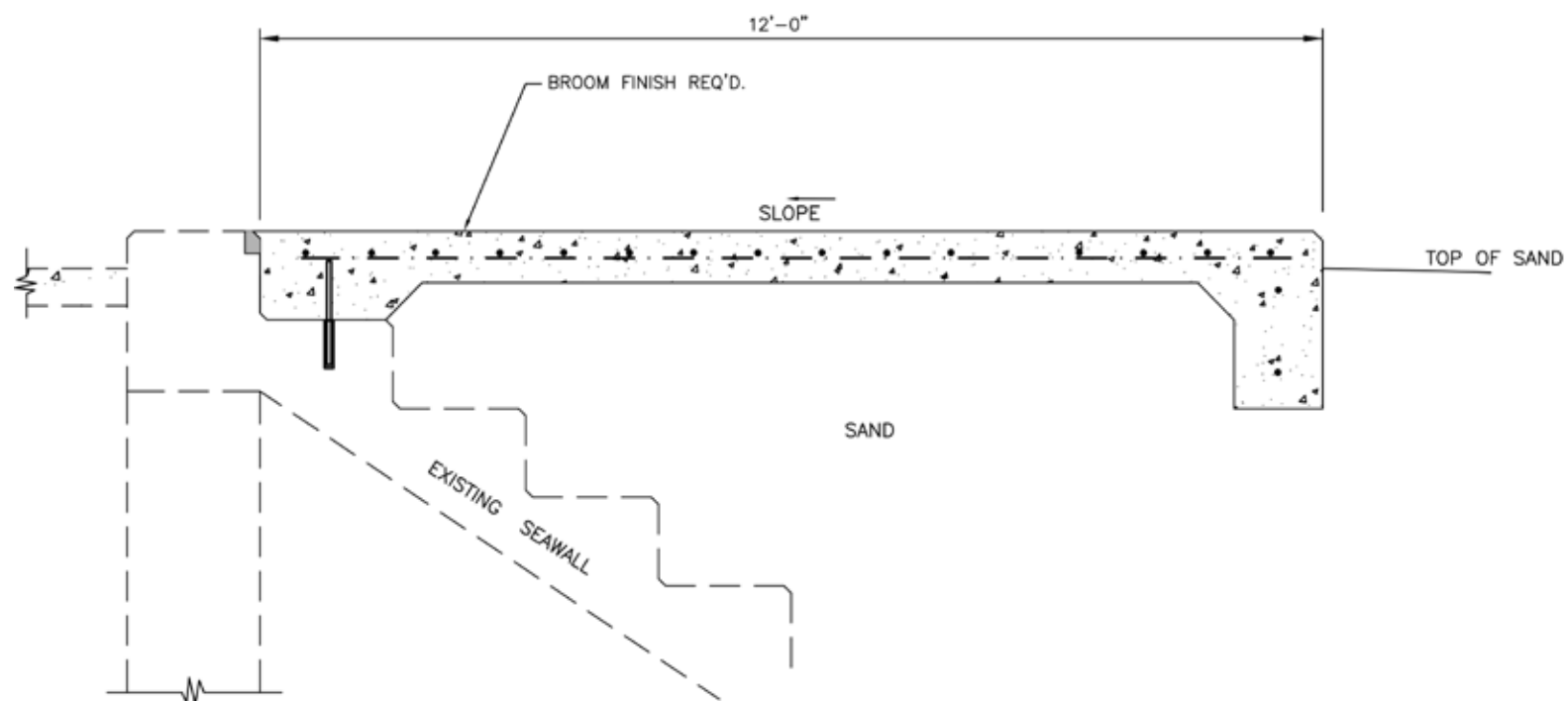




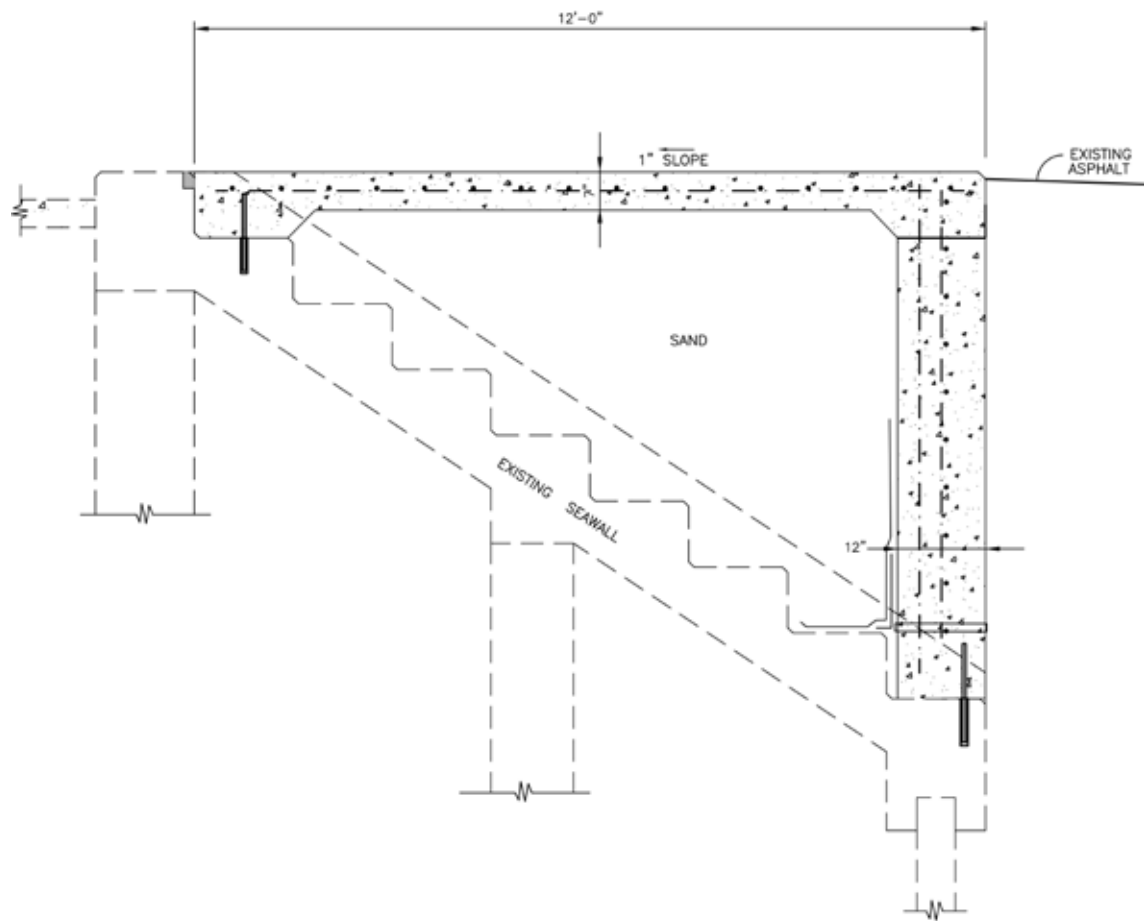
BOARDWALK TYPE 1 TYPICAL SECTION



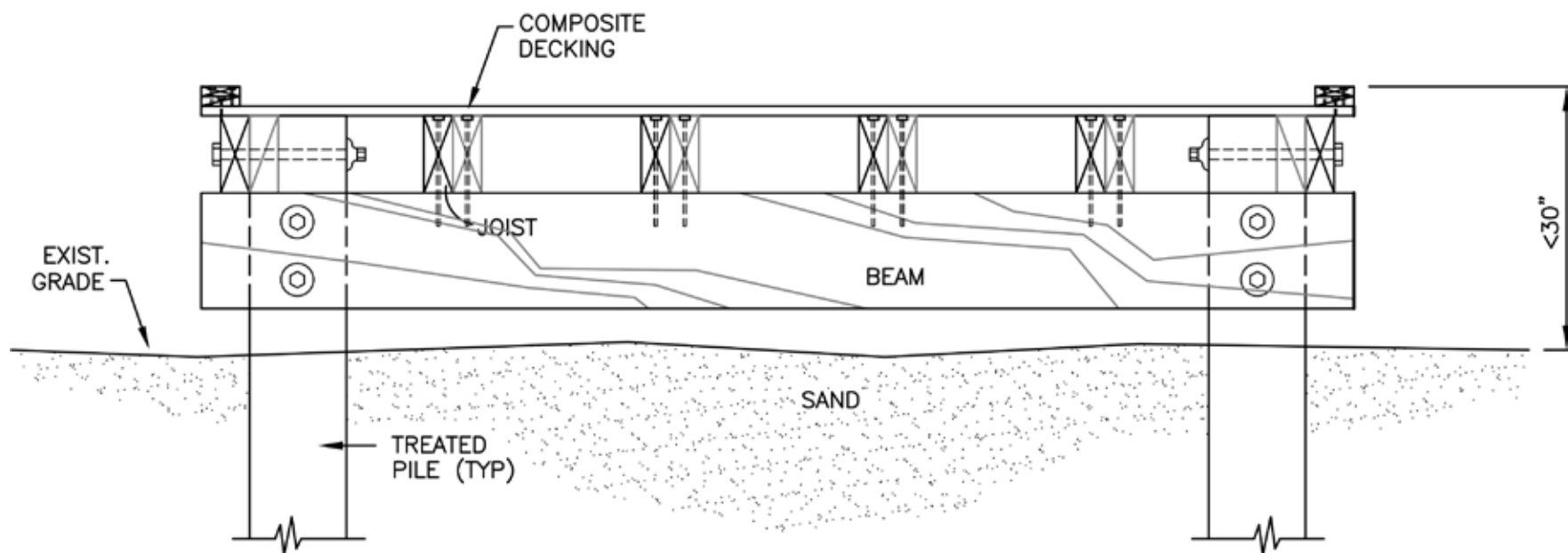
DETAIL B 7'-6" BILOXI SQUARE SIDEWALK



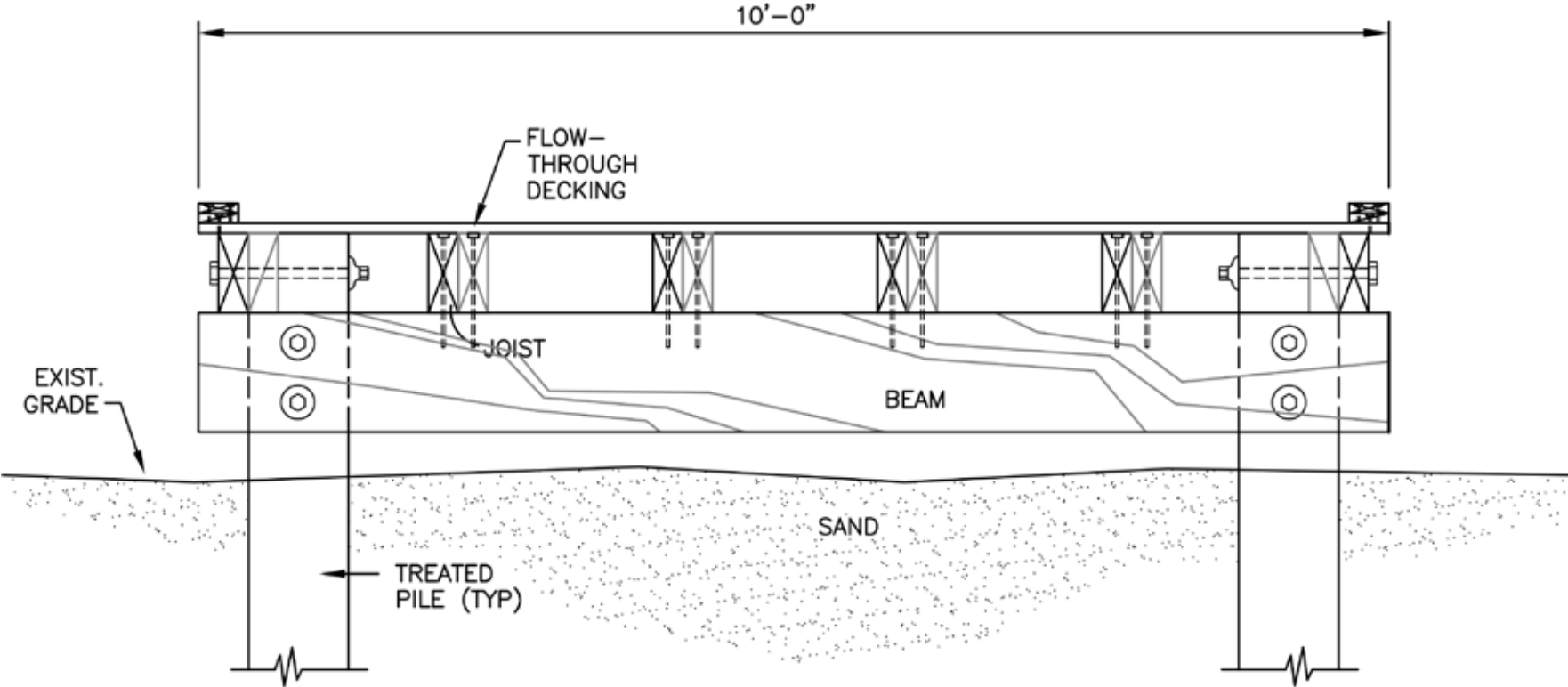
DETAIL
C 12' SIDEWALK AT SEAWALL



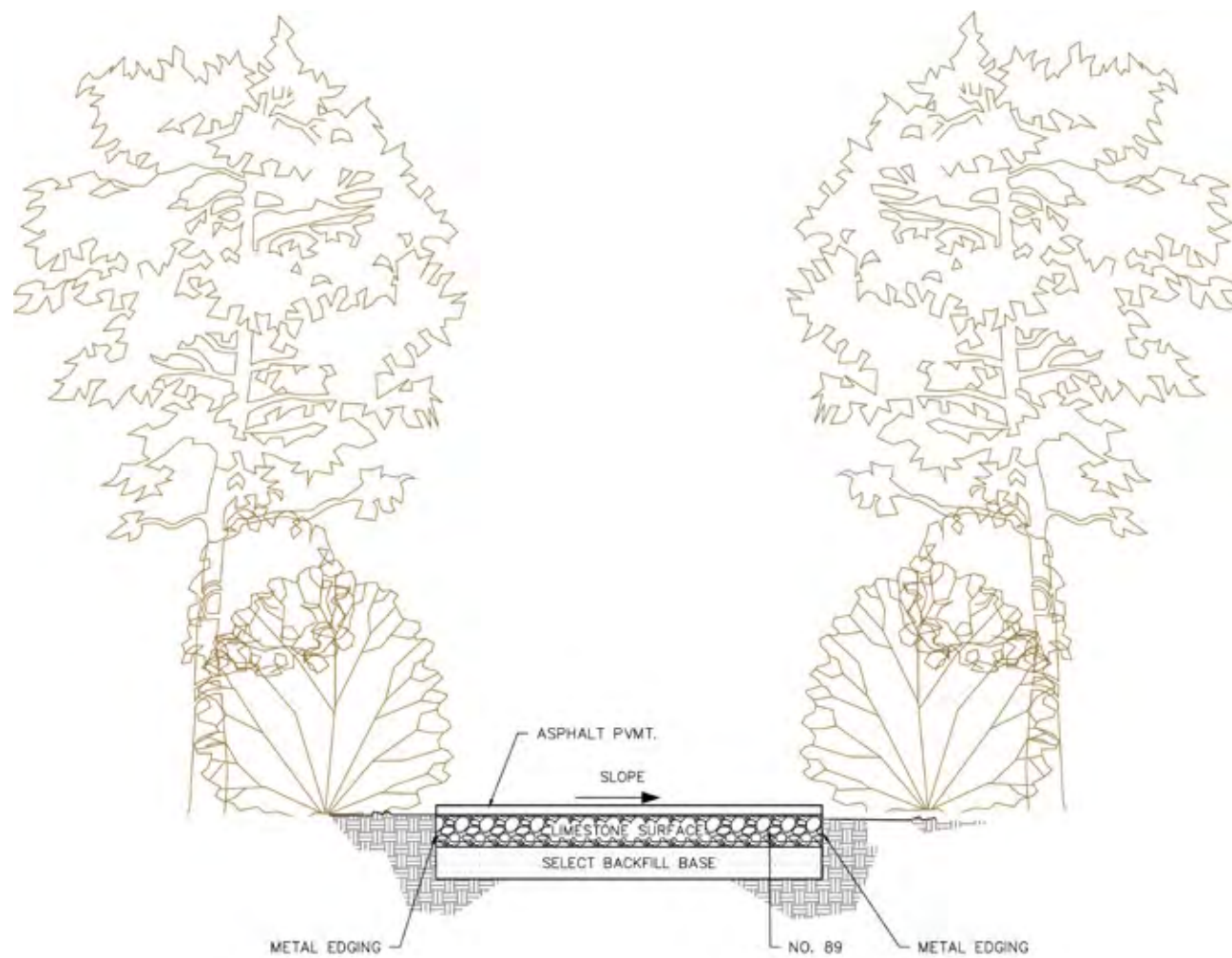
DETAIL D 12' ORIGINAL HARCO DESIGN (±2009)



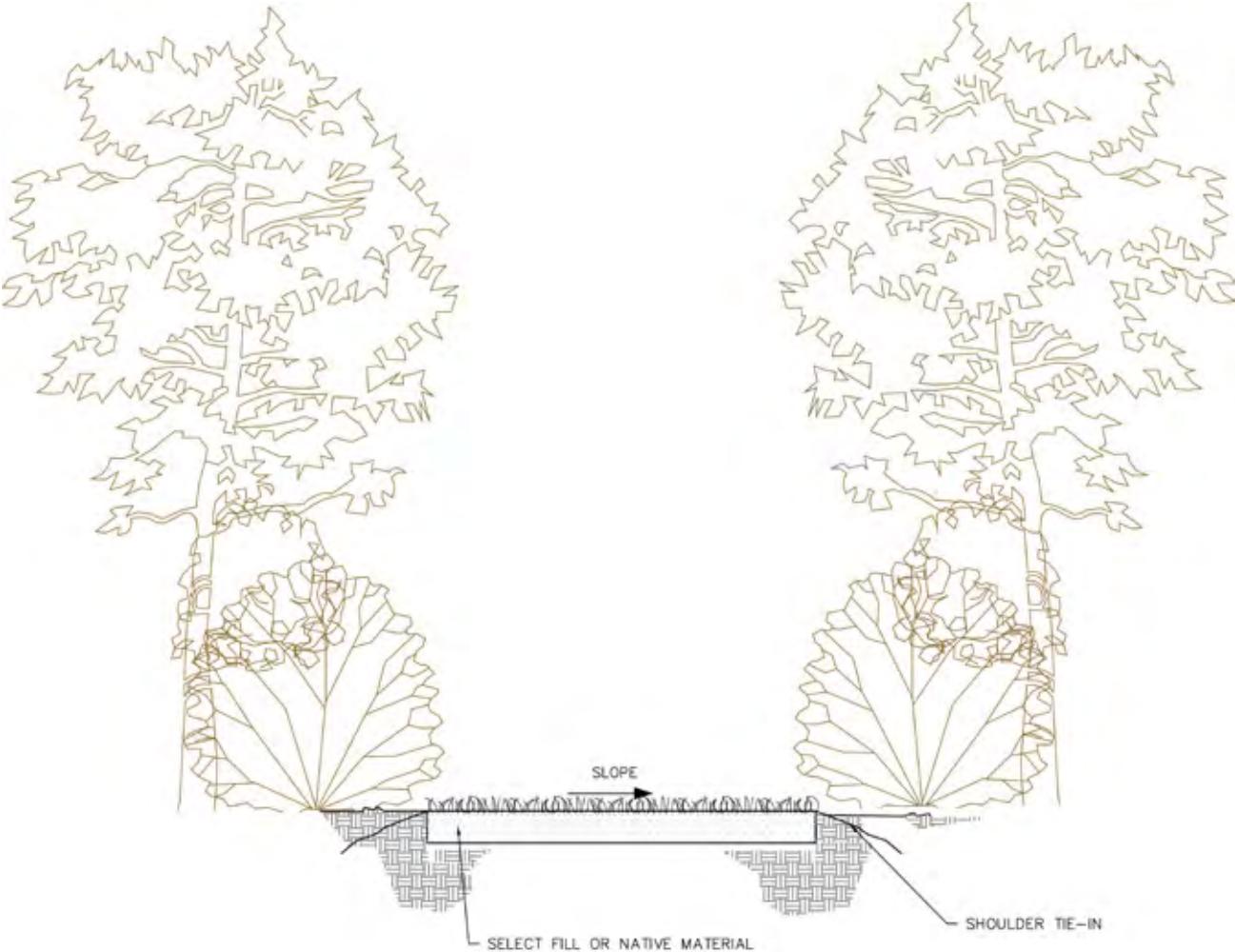
10' WEST BILOXI TIMBER BOARDWALK



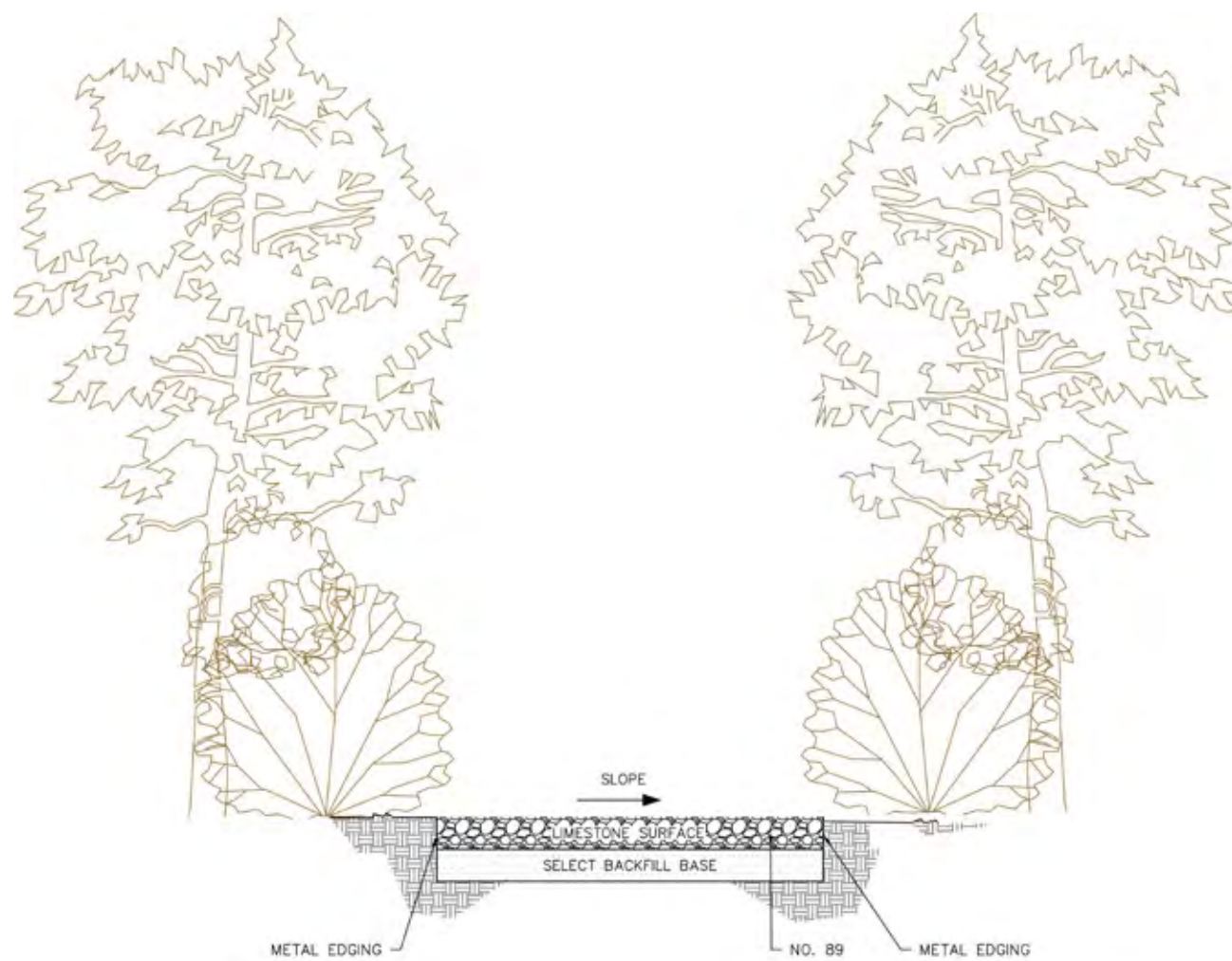
10' WEST BILOXI BOARDWALK W/ FIBER REINFORCED DECKING



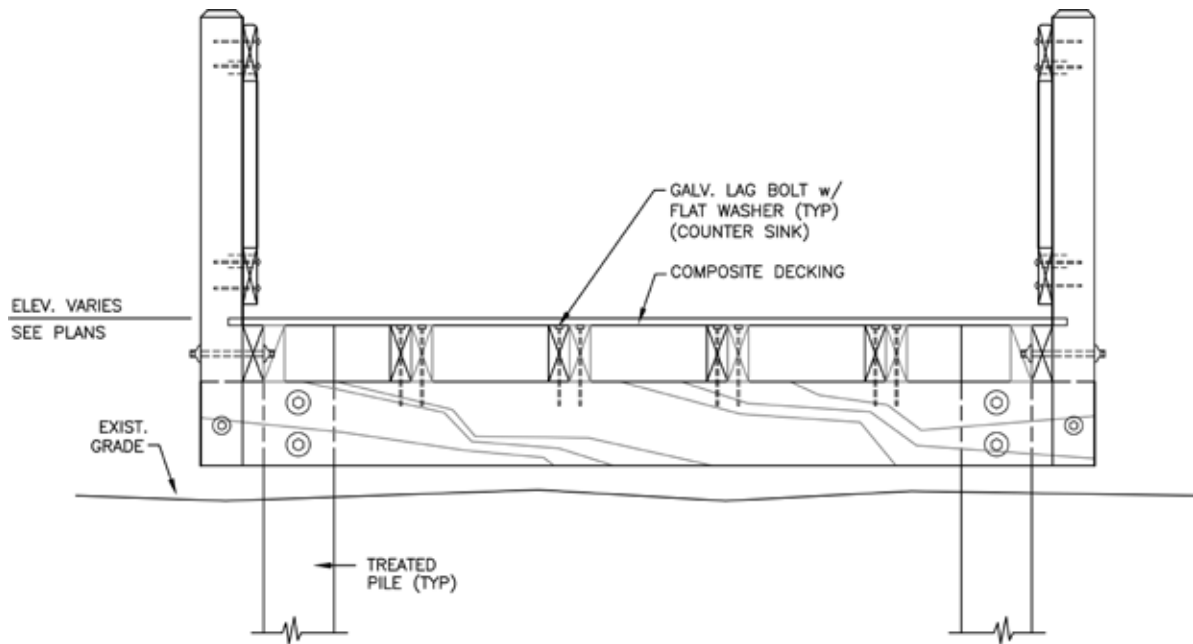
8' ASPHALT BIKE TRAIL



DETAIL H 8' GRASS TRAIL



DETAIL 1 8' LIMESTONE TRAIL



DETAIL J 8' TIMBER BOARDWALK



SIDES OF CURB RAMPS MAY BE RETURNED, PROVIDING USEFUL DIRECTIONAL CUES, IF PROTECTED FROM CROSS TRAVEL BY LANDSCAPING, STREET FURNITURE, POLES, OR EQUIPMENT.



GENERAL NOTES

1. THE DIMENSIONS AND SLOPES PRESENTED IN THE DETAILS ARE THE MINIMUM NECESSARY TO COMPLY WITH THE ADA AND MOST STANDARDS. ANY DEVIATION LESS THAN THE MINIMUM WIDTH OR GREATER THAN THE MAXIMUM SLOPE FROM THESE STANDARDS MUST BE DOCUMENTED WITH THE STANDARDS, BEING MET, TO THE GREATEST EXTENT PRACTICABLE AND CONSISTENT WITH THE NO CURRENT PUBLIC RIGHT-OF-WAY ACCESSIBILITY REQUIREMENTS.
2. CURB RAMPS, LANDINGS AND BLENDED TRANSITIONS MAY REQUIRE THE USE OF DETECTABLE WARNING. DETECTABLE WARNING ON THIS SHEET ARE SHOWN FOR ILLUSTRATION ONLY. REFER TO THE DETECTABLE WARNING DETAILS ON SHEET 4 OF 4 FOR DETAILS ON PLACEMENT, ORIENTATION & DIMENSIONS.
3. THE LOCATION, ORIENTATION, AND TYPE OF CURB RAMPS SHALL BE AS SHOWN IN THE PLANS.
4. ANY COMBINATION OF PERPENDICULAR, PERPENDICULAR RETURNED, AND PARALLEL CURB RAMPS MAY BE USED TO ACHIEVE AN ACCESSIBLE DESIGN AS LONG AS THE BASIC REQUIREMENTS FOR CURB RAMPS ARE MET.
5. CURB RAMPS SHALL BE PAID FOR AS SIDEWALK.
6. THE THICKNESS OF THE CURB RAMP SHALL BE A MINIMUM OF 4".
7. BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE OF 4" MINIMUM BY 4" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PAVEMENT. EXCEPT FOR CURB CUTS, THE CURB SHALL BE 4" MINIMUM ABOVE THE FINISHED GRADE.

CURB RAMP NOTES:

6. THE CLEAR WIDTH OF CURB RAMP RUNS SELECTING ANY FLARED SIDES, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE THE WIDTH OF THE SIDEWALK, 4' MINIMUM. THE RUNNING SLOPE OF A CURB RAMP SHALL BE 3% MINIMUM, AND 8.33% MAXIMUM (7.13% PREFERRED). THE RUNNING SLOPE OF BLENDED TRANSITIONS SHALL BE 5% MAXIMUM.
7. WHEN THE SLOPE OF THE ROADWAY EXCEEDS 8.33%, THE CURB RAMP LENGTH IS THE LENGTH NECESSARY TO MEET THE EXISTING SLOPE. IF IT IS NOT NECESSARY THAT THE RAMP EXCEED 15',
8. THE CROSS SLOPE OF CURB RAMPS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 2% MAXIMUM. 1% IS PREFERRED. AT PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL, AND AT MIDBLOCK PEDESTRIAN STREET CROSSINGS, THE CROSS SLOPE IS PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
9. GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMP RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF RAMP RUNS AND TURNING SPACES. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
10. RAMP TRANSITIONS BETWEEN WALKS, TURNING SPACES, LANDINGS, GUTTERS, OR STREETS SHALL BE FLUSH AND FREE OF ABRUPT VERTICAL CHANGES.
11. WHERE A PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP, FLARED SIDES SHALL BE SLOPED 3% MAXIMUM, MEASURED PARALLEL TO THE CURB LINE.
- TURNING SPACE NOTES:**
1. A TURNING SPACE 4' MINIMUM SHALL BE PROVIDED AT THE TOP OF PERPENDICULAR RAMPS AND AT THE BOTTOM OF RAMPS. RAMPS AND SPACES ARE ALLOWED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. IF THE TURNING SPACE IS CONTAINED BY A CURB WALL OR OTHER OBSTRUCTION, THE TURNING SPACE SHALL BE 4' MINIMUM BY 9' MINIMUM, WITH THE 9' DIMENSION PROVIDED IN THE DIRECTION OF TRAVEL TOWARD THE CONSTRAINT.
2. THE RUNNING SLOPE OF TURNING SPACES SHALL BE 2% MAXIMUM (1.5% PREFERRED). THE CROSS SLOPE OF TURNING SPACES SHALL BE 3% PREFERRED. AT PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL, AND AT MIDBLOCK PEDESTRIAN STREET CROSSINGS, THE CROSS SLOPE SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
3. BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE 4' MINIMUM BY 4' MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.
4. THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 3% MAXIMUM. 1% IS PREFERRED. THE CROSS SLOPE OF THE GUTTER OR STREET SHALL BE 2% MAXIMUM.



DETAIL
K-1 ADA RAMPS

		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		CURB RAMPS	
		RAMP DESIGN ELEMENTS	
			
		WORKING TITLE SOCCR - R SHEET NUMBER	
		SHEET 1 OF 4 FILENAME: SOCCR.DGN	

